



THE LONDON BOROUGH
www.bromley.gov.uk

BROMLEY CIVIC CENTRE, STOCKWELL CLOSE, BROMLEY BRI 3UH

TELEPHONE: 020 8464 3333

CONTACT: Philippa Gibbs

Philippa.Gibbs@bromley.gov.uk

DIRECT LINE: 020 8461 7638

FAX: 020 8290 0608

DATE: 7 April 2022

To: Members of the
DEVELOPMENT CONTROL COMMITTEE

Councillor Alexa Michael (Chairman)

Councillor Yvonne Bear (Vice-Chairman)

Councillors Vanessa Allen, Julian Benington, Katy Boughey, Peter Dean, Simon Fawthrop, Christine Harris, Colin Hitchins, Samaris Huntington-Thresher, William Huntington-Thresher, Charles Joel, Josh King, Tony Owen, Richard Scoates, Kieran Terry and Michael Turner

A meeting of the Development Control Committee will be held at Bromley Civic Centre on **TUESDAY 19 APRIL 2022 AT 7.30 PM**

PLEASE NOTE: This meeting will be held in the Council Chamber at the Civic Centre, Stockwell Close, Bromley, BR1 3UH. Members of the public can attend the meeting to speak on a planning application (see the box on public speaking below).

There will be limited additional space for other members of the public to observe the meeting – if you wish to attend, please contact us before the day of the meeting if possible, using our web-form:-

<https://www.bromley.gov.uk/CouncilMeetingNoticeOfAttendanceForm>

Please be prepared to follow the identified social distancing guidance at the meeting, including wearing a face covering.

TASNIM SHAWKAT

Director of Corporate Services & Governance

Public speaking on planning application reports is a feature at meetings of the Development Control Committee and Plans Sub-Committees. Members of the public wishing to speak will need to have already written to the Council expressing their view on the particular matter and have indicated their wish to do so to Democratic Services **by no later than 10.00 a.m.** on the working day before the date of the meeting.

The inclusion of public contributions, and their conduct, will be at the discretion of the Chairman. Such contributions will normally be limited to two speakers per proposal, one for and one against, each with three minutes to put their point across.

For further details, please telephone **020 8461 7743** or email to *committee.services@bromley.gov.uk*

A G E N D A

- 1 APOLOGIES FOR ABSENCE AND NOTIFICATION OF SUBSTITUTE MEMBERS**
- 2 DECLARATIONS OF INTEREST**
- 3 QUESTIONS FROM MEMBERS OF THE PUBLIC**

In accordance with the Council's Constitution, questions that are not specific to reports on the agenda must have been received in writing 10 working days before the date of the meeting – by Friday 1st April.

Questions specifically relating to reports on the agenda should be received within two working days of the normal publication date of the agenda. Please ensure that questions specifically relating to reports on the agenda are received by the Democratic Services Team by **5 pm on Monday 11 April 2022**.

- 4 CONFIRMATION OF THE MINUTES OF THE MEETING HELD ON 9 MARCH 2022**
(Pages 1 - 16)
- 5 (21/04851/FULL1) - WEST WICKHAM LIBRARY, GLEBE WAY, WEST WICKHAM, BR4 0SH** (Pages 17 - 74)

West Wickham ward

- 6 HERITAGE AT RISK UPDATE** (Pages 75 - 84)

The Council's [Local Planning Protocol and Code of Conduct](#) sets out how planning applications are dealt with in Bromley.

DEVELOPMENT CONTROL COMMITTEE

Minutes of the meeting held at 7.30 pm on 9 March 2022

Present:

Councillor Alexa Michael (Chairman)
Councillor Yvonne Bear (Vice-Chairman)
Councillors Vanessa Allen, Julian Benington, Katy Boughey,
Peter Dean, Simon Fawthrop, Christine Harris, Colin Hitchins,
Charles Joel, Josh King, Tony Owen, Kieran Terry,
Pauline Tunnicliffe and Michael Turner

Also Present:

Councillor Christopher Marlow

91 APOLOGIES FOR ABSENCE AND NOTIFICATION OF SUBSTITUTE MEMBERS

Apologies were received from Councillor William Huntington-Thresher, Councillor Samaris Huntington-Thresher and Councillor Richard Scoates. Councillor Pauline Tunnicliffe attended as alternate for Councillor William Huntington-Thresher.

92 DECLARATIONS OF INTEREST

There were no additional declarations of interest.

93 QUESTIONS BY MEMBERS OF THE PUBLIC ATTENDING THE MEETING

Four questions for oral reply and four questions for written reply were received. These are attached at **Appendix A**.

94 CONFIRMATION OF THE MINUTES OF THE MEETING HELD ON 11 JANUARY 2022

The minutes of the meeting held on 11 January 2022, were agreed and signed as a correct record.

95 (20/04838/FULL1) - UNIT 2A FARNBOROUGH WAY, ORPINGTON, BR6 7DH

Description of application - Demolition of existing buildings on site. Redevelopment to provide a food store (Class E) and associated access, car parking, and landscaping works.

The Planning Officer gave a brief presentation, providing an overview of the amendments proposed and update on the report.

Oral representations in support of the application were received from the applicant who gave the following responses to Member questions:-

- The pedestrian access from residential roads had been closed off as a result of consultation with local residents but if necessary, pedestrian access could be opened up.
- It was not considered that a new Aldi Store would have a significant impact on the existing local stores in Farnborough Village. The main competition would be with other supermarkets in the locality.
- It was considered that traffic through Farnborough Village would be limited and although there may be a modest increase, there would not be a significant impact on the Village. The figures for traffic through the Village (accepted by TfL) were 22 (weekday peak) and 37 (Saturday peak). Calculations had indicated that in any one hour there would be 70 departures and 70 arrivals to the store (with 22 of these travelling through the Village).
- For traffic heading south from Bromley, it would not be possible to turn right into the store, so traffic would have to make a u-turn at the roundabout to access the store.
- A large proportion of staff, if not all, would be recruited from the local area and would therefore not be parking in the local area. The store was also on a bus and cycle route. While some staff may drive to the store, this would be a small number and it would be wrong to assume that all staff had access to a car.

Oral representations in objection to the application were received from a neighbour.

Councillor Marlow, local Ward Councillor, addressed the Committee explaining that, in his view, the Officer's report provided an excellent assessment of the application, and he supported the Officer recommendation of refusal for 4 key reasons:

1. The application was balanced in terms of support and opposition. However, it was noticeable that those opposed to the application provided extensive details of the grounds for their opposition, while no such detail was provided by those in support. It was also noticeable that residents in support of the application lived on the other side of the A21 and were therefore less affected by the proposals.
2. A number of residents had expressed concerns around traffic to the site.
3. TfL had proposed a pedestrian walkway to residential roads but local residents had expressed concerns about this proposal on the grounds of safety.
4. Due to the current policies being pursued by TfL, it was unlikely that a new bus route would be introduced. Consequently, the site was likely to remain car dependant.

As a result of the above, Councillor Marlow encouraged the Committee to support the Officer recommendation and refuse the application.

Committee Member and Ward Member Councillor Joel thanked the Members of the Committee who had attended the site visit. All three Ward Members supported the Officer recommendation of refusal. It was the view of the Ward Members that the building would be detrimental to the area due to the size, mass and bulk, together with the traffic movement in and out of the car park. It was also noted that the roots of the large oak tree could be damaged during the land excavation to create the lower ground floor steel sheeting. There would also be a gap in the central island in Farnborough Way and visitors to the site may use this to turn right when coming out of the car park. There appeared to be no provision for waste/food disposal bins or storage on the plans. The proposed location of the manager's office, meeting room and staff room, together with the cycle racks on the lower ground floor, may allow employees to make use of the side door access, with staff perhaps parking their cars in nearby roads. Although it could be a condition with any consents for foul and surface water disposals, it was the view of Councillor Joel that these should have been shown in the plans. Consequently, Councillor Joel supported the Officer recommendation and moved that the application be refused. Councillor Fawthrop seconded the motion for refusal.

Councillor Benington explained that he had lived in Farnborough Village for a number of years and was therefore familiar with the Village. The proposed opening hours (8am to 10pm) were long and would be a disturbance to local residents. Delivery times were also long and there would be consequent noise affecting local residents. It was unlikely that staff would cycle to work and as a result of shift patterns, the majority of staff were in fact likely to travel by car. As a result, Councillor Benington supported refusal.

Councillor Terry queried how, with the pedestrian access now closed, local roads would be congested with parked cars. It was also noted that market competition was not a material planning consideration. There were a number of benefits to the application such as increased employment, greater choice and reduced travel to other supermarkets.

Members having considered the report and objections, RESOLVED that PERMISSION BE REFUSED as recommended, for the reasons set out in the report of the Assistant Director, Planning and the additional reason:-

1. The proposal would add to traffic in the locality.

96 (21/03190/FULL1) - THE PRINCESS ROYAL UNIVERSITY HOSPITAL, FARNBOROUGH COMMON, ORPINGTON, BR6 8ND (FARNBOROUGH AND CROFTON)

Description of Application - Erection of a 197 spaces parking deck.

The Planning Officer gave a brief presentation, providing an overview of the application and update on the report.

Oral representations in support of the application were received from the applicant who gave the following responses to Member questions:-

- The existing surface level car park would be realigned to provide 195 shared parking spaces with 40 electric vehicle charging points.
- An additional parking deck further than the current proposal could provide more parking spaces. However, a further floor would be level above the existing tree lines and would create disturbance to neighbours on Barkway Drive which did not exist with the current proposal. There was existing accommodation for staff at two locations and staff had indicated that they preferred to leave the site for their rest breaks.
- An acoustic assessment had been undertaken and had passed due consideration. A single deck was being proposed. The deck would be fully constructed off site with in-situ construction estimated to take around 90 days. The foliage around the extremity of the site would also mitigate against noise.
- If approved, the car park would be fully constructed and open by November 2022.

The Chairman reported that she had received an email in support of the application from Councillor David Jefferys, Chairman of the Health and Wellbeing Board.

Committee Member and Ward Member Councillor Joel reported that the total figure of 773 spaces included 51 spaces for the proposed new Endoscopy Unit that had not yet been given approval under Town and Country Planning Acts. The Council had a policy to allow on-street parking in areas throughout the Borough and the surrounding roads in close vicinity of the hospital. In the view of Councillor Joel, this would not change once the new car park deck had been completed. A number of concerns had been raised by local residents concerning the impact of flood lighting, car headlights and the need to ensure that additional landscaping was provided to the perimeter of the site. The Planning Department were encouraged to take all measures to ensure that the points were fully addressed. Councillor Joel supported the Officer recommendation and moved that approval be granted. The motion for approval was seconded by Councillor Peter Dean.

The Chairman expressed the view that the application would assist with parking provision and reduce pressure on parking in residential roads near the hospital. She therefore welcomed and supported the application.

Councillor Fawthrop suggested that an additional condition for swift bricks and sustainability measures should be included, and it was agreed these would be included in the future endoscopy unit application.

Members having considered the report, objections and representations, RESOLVED that PERMISSION BE GRANTED as recommended and subject to the conditions and informatives set out in the report of the Assistant Director, Planning.

It was further RESOLVED that a condition be added regarding renewable energy on site to be agreed between the Hospital Trust and Officers.

97 (21/03622/FULL1) - BURNT ASH HEIGHTS, PIKE CLOSE, BROMLEY, BR1 5BN (PLAISTOW AND SUNDRIDGE)

Description of Application - Demolition of existing buildings and phased redevelopment comprising 170 residential units in buildings ranging from 2 to 13 storeys. Associated landscaping, car and cycle parking and ancillary development

The Planning Officer gave a brief presentation, providing an overview of the application and update on the report. The applicant had agreed the outstanding heads of terms, the bus contribution had been paid and the condition concerning the pedestrian crossing had been amended to require the pedestrian crossing improvements being carried out within 2 years of completion of the development.

Oral representations in objection of the application were received from a local resident.

Oral representations in support of the application were received from the agent who gave the following responses to Member questions:-

- The points that had been raised concerning overlooking were fully understood and the current proposals were the optimum solution to the competing challenges of delivering units and protecting residential amenity.
- Time and effort had gone into ensuring the flow of the building and protecting the landscape and environment.
- A variety of measures were being applied in terms of renewable energy. The cost of living for residents would be significantly less.
- The proposals would meet electric charging requirements.
- The number of additional units being proposed ensured the viability of the scheme.
- The scheme was consistent with Tall Building Policy. Different permutations had been considered and the proposals before the Committee were the optimum balance.

The Chairman reported that she had received an email from Ward Councillor Peter Morgan in support of the application.

Committee Member and Ward Councillor Turner reported that while he supported redevelopment of the site, he was not in favour of the current

proposals which included more high-rise building. While there had been a ballot of residents and the majority had been in favour of redevelopment, they had not been in favour of this particular scheme. The proposals represented an overdevelopment of the site and many grounds of objection had been put forward. The proposals replaced one 13-storey block with a number of other high-rise blocks. There was insufficient parking and this would burden surrounding roads which were already under pressure. It was also disappointing that there was no increase in social housing units. Councillor Turner also highlighted that the proposal did not accord with London Policy D9. Consequently, Councillor Turner moved refusal on the grounds of overdevelopment, visual impact on surrounding areas and failure to comply with London Policy D9. Councillor Owen seconded the motion for refusal.

Councillor Peter Dean recognised the constraints of developing urban areas and while acknowledging the loss of amenity, there was a shortage of housing supply and this was the overriding factor. As such Councillor Dean moved approval. The motion was seconded by Councillor Christine Harris.

In response to a question from Councillor Terry, Officers confirmed that this site was not included in the 5-year housing land supply which had included only deliverable sites (i.e. those with planning permission) and at the time Officers had not been in a position to include this site.

The Planning Officer reported that, were the Committee minded to approve the application, an energy condition would be included. The Planning Officer also confirmed that a Biodiversity condition was recommended and that the wording should be amended to require the condition details prior to 'above-ground works'. The Planning officer also clarified that any permission granted would be subject to any additional conditions as advised by the Assistant Director of Planning and Building Control.

Councillor Fawthrop requested that an additional condition concerning ridge height be added.

Members having considered the report, objections and representations, RESOLVED that PERMISSION BE GRANTED SUBJECT TO LEGAL AGREEMENT AND ANY DIRECTION BY THE MAYOR OF LONDON as recommended and subject to the conditions and informatives set out in the report of the Assistant Director, Planning.

It was further RESOLVED that the following additional conditions be added:

- 1. Energy**
- 2. Ridge height**

**98 (21/04667/FULL1) - 62 HIGH STREET BROMLEY BR1 1EG
(BROMLEY TOWN)**

Description of Application - Proposed conversion of existing building and 3-storey roof extension to accommodate Class E commercial space on the ground floor and 30 residential flats on the upper floors. Cycle and refuse storage to be provided at ground floor level.

The Planning Officer gave a brief presentation, overview of the application and update on the report.

Oral representations in support of the application were received from the applicant who gave the following responses to Member questions:-

- Due to the constrained town centre site, it was very difficult to accommodate disabled parking spaces. Census data demonstrated that car ownership for people in a wheelchair living in a town centre location was relatively low.
- The current empty unit had been marketed for some time and had received approaches from the likes of second-hand stores. The proposals before the Committee were about sustaining the high street.

The Chairman reported that she had reviewed the application which met what the Council was looking for in planning terms. The proposed design was in keeping with the existing building and 30 units was about right for the site, although it was regrettable that there were no affordable units. The Chairman moved that permission be granted. The motion was seconded by Councillor Fawthrop who expressed disappointment that about the lack of disabled parking and requested an additional condition concerning ridge height.

Councillor Allen noted the emails from Bromley Civic Society that had previously been circulated and expressed the view that the way the original building design had been preserved worked well.

Members having considered the report, objections and representations, RESOLVED that PERMISSION BE GRANTED SUBJECT TO A LEGAL AGREEMENT as recommended and subject to the conditions and informatives set out in the report of the Assistant Director, Planning.

It was further RESOLVED that a condition be added concerning ridge height.

**99 CONFIRMATION OF ARTICLE 4 DIRECTIONS TO REMOVE PART 1, CLASS B AND C PERMITTED DEVELOPMENT RIGHTS IN PETTS WOOD ASRC AND CHISLEHURST ROAD CONSERVATION AREA
Report HPR2022/012**

The report recommended that the Council confirmed two non-immediate Article 4 Directions to withdraw permitted development (PD) rights which

allowed various alterations to the roof of a dwellinghouse without planning permission. These Directions would withdraw Part 1, Class B and C PD rights in the Petts Wood Area of Special Residential Character as shown in the Bromley Local Plan (January 2019); and withdraw Part 1, Class C PD rights in the Chislehurst Road Conservation Area (based on the boundary prior to recent changes). The Direction would replace an existing Direction which removed these PD rights on front roof slopes only. In line with the requirements of legislation, representations on the proposed Directions were sought. The Council must take into account any representations made before it confirmed the Article 4 Directions. No representations were received. If confirmed, the Directions would come into force on 19 July 2022.

The Committee noted that the new Conservation Areas (The Thrifts and The Covert) were covered by the Direction. The Head of Planning Policy and Strategy also confirmed that the Chenies Conservation Area was already covered by a similar Direction.

RESOLVED: That

- 1. The confirmation of two non-immediate Article 4 Directions to withdraw Part 1, Class B and C PD rights in the Petts Wood Area of Special Residential Character; and withdraw Part 1, Class C PD rights in the Chislehurst Road Conservation Area be endorsed. These PD rights are currently granted by the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) (“the GPDO”). The areas covered by the Directions are shown on the maps at Appendix 1 and Appendix 2.**
- 2. The making of a direction to cancel the existing Article 4 Direction which removes Part 1, Class B and C PD rights on front roof slopes in the Petts Wood ASRC be endorsed. The timing of this cancellation will be aligned with the date when the new direction comes into force, so as to avoid any gap in coverage of the directions.**
- 3. The Committee notes that the matter will be considered by the Renewal, Recreation and Housing Policy Development and Scrutiny Committee for pre-decision scrutiny.**

**100 CONFIRMATION OF SIX ARTICLE 4 DIRECTIONS TO REMOVE PERMITTED DEVELOPMENT RIGHTS FOR USE CLASS E TO RESIDENTIAL USE
Report HPR2022/011**

The report recommended that the Council confirmed six non-immediate Article 4 Directions to withdraw permitted development (PD) rights which allowed premises in Use Class E to change to residential use. These Directions applied to three Business Improvement Areas and three Office Clusters as set out in the Bromley Local Plan. The Directions would replace existing Directions which removed office to residential PD rights in these

areas; these existing Directions would lapse on 31 July 2022. In line with the requirements of legislation, representations on the proposed Directions were sought. The Council must take into account any representations made before it confirms the Article 4 Directions. No representations were received on the six proposed Directions. If confirmed, the Directions would come into force on 27 July 2022.

RESOLVED: That

- 1. The confirmation of six non-immediate Article 4 Directions to withdraw Part 3, Class MA PD rights in Bromley's Business Improvement Areas and Office Clusters, as designated in the Bromley Local Plan and shown at Appendix 1 be endorsed. These PD rights are currently granted by the Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended) ("the GPDO").**
- 2. The Committee notes the matter will be considered by the Renewal, Recreation and Housing Policy Development and Scrutiny Committee for pre-decision scrutiny.**

101 HOUSING DELIVERY TEST UPDATE
Report HPR2022/013

The report provided an update on the 2021 Housing Delivery Test results and the implications for Bromley. The results triggered the requirement to prepare an action plan to assess the causes of under-delivery and identify actions to increase delivery in future years; this should be published within six months of the 2021 Housing Delivery Test results being published (i.e. by July 2022).

RESOLVED: That the report be noted.

102 DELEGATED ENFORCEMENT ACTION (OCTOBER 2021 TO DECEMBER 2021)
Report HPR2022/014

The report set out the enforcement action authorised under delegated authority for alleged breaches of planning control.

Members noted that an additional table (setting out the information in Ward order with a glossary) had been circulated and published on the website.

The Committee requested that in future the recommendation column should appear in plain English to make it more understandable for members of the public reading the report.

Councillor Terry highlighted that there had been some delays with enforcement of the Untidy Site Notice for Victoria Road, Chislehurst. The

Assistant Director for Planning and Building Control agreed to look into the matter further and provide an update following the meeting.

RESOLVED: That the report be noted.

The Meeting ended at 9.43 pm

Chairman

**DEVELOPMENT CONTROL COMMITTEE
9 MARCH 2022**

THE FOLLOWING QUESTIONS HAVE BEEN SUBMITTED FOR ORAL REPLY

1. From Ms Alisa Igoe

The Ashton Fire report was filed 24 February 2022 yet is not included in the documentation on the agenda. Whilst it reports the HSE are satisfied with the review, it also highlights some issues that concern me. Could you please confirm that the Council will request a copy of the management procedures for the fire vehicle access route, monitor them frequently and call on Ashton Fire and Riverside Housing to rectify immediately and permanently any difficulties with the access route for fire vehicles being obstructed.

Reference:

Page 39, 6.2.48

Pike Close – Ashton Fire response to HSE Review Page 2, 2.2

The Health & Safety Executive comment:

"It is noted that the proposed buildings will contain 178 (now 170) apartments. However, the number of car parking spaces appears substantially less than this number. Therefore, there may be potential for cars to be parked in places which would obstruct access for fire appliances.

Additionally, it is highly likely that the fire service pre-determined attendance of fire appliances to a fire in the proposed buildings will include 'high-reach' fire appliances. It is therefore recommended that access routes be of sufficient width to accommodate such appliances. Resolving this issue may affect land use planning considerations such as landscaping and car parking provision."

Ashton Fire response 24 February 2022:

"HSE comment is noted. However, the car parking spaces arrangement is outside of the Building Regulation 2010, Part B life safety functional requirements scope. The fire strategy notes the requirement for the FRS vehicle appliances and the access route shall be kept unobstructed. Suitable management procedures shall be developed to keep the FRS access route unobstructed."

Reply:

The proposed design meets with planning requirements and will be further considered under the Building Regulations at Building Control stage.

Under the current legislation, once the building is occupied, London Fire Brigade are the enforcing body under The Regulatory Reform (Fire Safety) Order 2005. This includes for the maintenance of adequate access for fire fighting vehicles.

Supplementary Question:

There needs to be further verification of whether the existing fire hydrants are operational. If they are found not to be operational can the landlord be forced to remedy?

Reply:

It is essential that the fire hydrants work, and the Council will push to ensure that they are operational.

2. From Ms Alisa Igoe

As soon as I hear the term "closed window solution" I immediately think of the hot summers in the UK and people's need to open doors and windows. Will the Council be requiring, as a condition of planning approval, a full ventilation strategy from the developer, for all housing units on the estate, this especially as the report says "they do not appear to have fully considered the ventilation strategy at this stage" and this despite mentions of environmental noise in excess of guidelines and road traffic pollution as the site is within the Area Quality Management Area.

Reference 6.4.3

The scheme does not propose any north facing single aspect units. However, Blocks A, B and C would all include single aspect units whose sole outlook would be onto Burnt Ash Lane, including from bedroom windows .. the residents of these flats would be unable to open windows or have access to a private amenity space without an unacceptable impact from noise. Given the site's location in the Air Quality Management Area, residents may also be subjected to high levels of road traffic pollution."

Reference 6.4.4

An Environmental Noise Assessment was carried out by the IDOM dated July 2121. Due to the elevated noise climate, the noise report recommends a "closed window" solution should be provided for the majority of the units across the development together with alternative means of ventilation. Whilst the applicant has considered the use of acoustic glazing for reducing noise, they do not appear to have fully considered the ventilation strategy at this stage. It is not clear for example how the trickle vents are being acoustically treated in order to not become a weak point."

Reply:

The requirements for ventilation are covered under Building Regulations and will be considered at Building Control stage.

Supplementary Question:

The use of trickle vents will result in more windows having to be open and there more appropriate ventilation is available to reduce noise. Will there be consideration of noise mitigation measures associated with the site?

Reply:

These issues come under Building Regulations and a response can be provided at that stage if the Committee is minded to approve the application.

3. From Mr Tony McPartlan

Re: Agenda Item 7 - Pike Close - Will all existing tenants be moved into a new property which is at least the same size as their current one?

Reply:

The unit mix of the reprovided 92 affordable homes has also been derived from a housing needs assessment undertaken by Riverside. This study considers the existing and future needs of residents with regards to unit sizes and provides a mix of unit sizes that respond to the existing need of residents on the estate, but also is reflective of the identified local need and site context.

All tenants have a right to remain on the estate and will be offered a new home that meets their housing need.

Existing tenants will keep the same tenancy rights and pay the same levels of rent.

Supplementary Question:

In a ballot in April 2019, the majority of residents voted in favour of redevelopment, although they did not necessarily vote in favour of what is currently being proposed. How close are these proposals to the ballot proposals?

Reply:

The Committee will discuss this when the application is determined.

4. From Mr Tony McPartlan

Re: Agenda Item 7 - Pike Close - Section two of the report details four construction phases lasting a total of nine years. What is the estimated timeline for each of the four phases?

Reply:

The scheme will be delivered across four separate phases which is in line with a single decant strategy for the existing residents of the estate

Assuming the application is approved, construction of Phase 1 is anticipated to start in April 2022 and, completion of Phase 4 in 2031

Supplementary Question:

Communication, or lack of communications, is an issue that is raised regularly. What can be done to ensure that Riverside effectively communicate with residents?

Reply:

The Chairman suggested that the point could be considered when the application was determined.

THE FOLLOWING QUESTIONS HAVE BEEN SUBMITTED FOR WRITTEN REPLY

1. From Mr Ankur (Anx) Patel

What ways are Bromley Council's environmental assessments in planning being updated in accordance with new research and aligned with the aims of the UK government and COP26?

Reply:

Bromley have commenced a Local Plan review and as part of this work will need to address the requirements of legislation and Government planning policy and guidance. At this stage, it is too early to say what evidence will be required to inform the Local Plan review, but clearly, where evidence is explicitly required as part of the Local Plan process, this will need to be addressed in order for any new Local Plan to be found sound.

2. From Mr Clive Lees, Chairman, Ravensbourne Valley Residents

With regard to 2A Downs Hill, is the Council intending to take legal action regarding the previously identified issue that TPO trees were cut down without permission?

Reply:

The officer expediency assessment is pending conclusion. We are working with the landowner to secure replacement planting.

One of the trees planted has failed but is proposed to be replaced by the end of March. If this doesn't take place, it would impact the expediency assessment, so officers will not be concluding the case until April 2022.

3. From Ms Carol Pitman

How is the housing target monitored, and what number of dwellings have been completed for the 2019/20 -2028/29 target period, for the whole of the borough, and for also for the BR6 postcode, as at 28 Feb 2022?

Reply:

Details of housing delivery in Bromley are available on the Department of Levelling Up, Housing and Communities website -

<https://www.gov.uk/government/statistics/housing-supply-net-additional-dwellings-england-2020-to-2021>. These figures are for the whole borough; they are not broken down by postcode.

4. From Ms Carol Pitman

What is the number of dwellings which are planned to be completed for the 2019/20 -2028/29 housing target period, for the whole of the borough, and for also for the BR6 postcode, after 28 Feb 2022?

Reply:

Past and projected housing delivery in Bromley is set out in the Bromley Housing Trajectory. The latest trajectory was published in November 2021 and can be accessed here:

https://www.bromley.gov.uk/download/downloads/id/6455/bromley_housing_trajectory.pdf. The trajectory figures are borough-wide and are not broken down by postcode.

The trajectory shows delivery of 536 units in 2019/20, with projected delivery of 325 in 2020/21 (as completion figures were not available at the time of writing). This gives a total of 861 units, which is less than the cumulative housing target for these two years (1,098 units).

The trajectory has a year by year breakdown of delivery for the period 2021/22 to 2025/26, but beyond this delivery is grouped into five year periods and is not broken down by year. The trajectory projects future delivery of 3,245 units between 2021/22 and 2025/26, with further projected delivery of 5,614 units between 2026/27 and 2035/36. In total, the trajectory projects delivery of 9,569 units between 2021/22 and 2035/36, which is less than the cumulative housing target for this period (11,804 units).

This page is left intentionally blank

Agenda Item 5

| | | | |
|---|---|---|---------------------------------|
| Committee Date | 19/04/22 | | |
| Address | West Wickham Library Glebe Way West Wickham BR4 0SH | | |
| Application Number | 21/04851/FULL1 | Officer - Catherine Lockton | |
| Ward | West Wickham | | |
| Proposal | Refurbishment and extension to West Wickham library including a new cafe and community space; redevelopment of the car park site on Croft Avenue to provide a mews of 6 houses comprising 12 flats (6 x 1 bed and 6 x 3 bed) and a detached three storey block of flats comprising 14 units (3 x studio and 11 x 1 bed); with associated bike and bin stores, car parking, ancillary space and private and communal amenity space and alterations to the access road. | | |
| Applicant Deborah Wood London Borough of Bromley Council | | Agent Sheila Eilenberg Brimelow McSweeney Architects | |
| Bromley Civic Centre Stockwell Close Bromley BR1 3UH United Kingdom | | 26 Great Queen Street Covent Garden London WC2B 5BL | |
| Reason for referral to committee | LBB Application | | Councillor call in NO |

| | |
|-----------------------|---|
| RECOMMENDATION | PERMISSION subject to a transfer of funds |
|-----------------------|---|

| |
|---|
| KEY DESIGNATIONS Biggin Hill Safeguarding Area London City Airport Safeguarding Area Secondary Shopping Frontage Open Space Deficiency Smoke Control SCA 51 |
|---|

| |
|-------------------------|
| Land use Details |
|-------------------------|

| | Use Class or Use description | Floor space (GIA SQM) |
|----------|---|----------------------------------|
| Existing | Public Library (Class F1(d)) | 489sq.m. |
| Proposed | Public Library (Class F1(d)) Restaurant/café (Class E(b)) Residential | 700sq.m. 96sq.m. 1556sq.m. |

Residential Use – See Affordable housing section for full breakdown including habitable rooms

| | Number of bedrooms per unit | | | | |
|-------------------------------|-----------------------------|---|---|--------|-------------------------|
| | 1 | 2 | 3 | 4 Plus | Total / Payment in lieu |
| Market | 6 | 0 | 6 | 0 | 12 |
| Affordable (shared ownership) | 0 | 0 | 0 | 0 | 0 |
| Affordable (social rent) | 14 | 0 | 0 | 0 | 14 |
| Total | 20 | 0 | 6 | 0 | 26 |

| Vehicle parking | Existing number of spaces | Total proposed including spaces retained | Difference in spaces (+ or -) |
|------------------------|---------------------------|--|-------------------------------|
| Standard car spaces | 65 | 10 | -55 |
| Disabled car spaces | 2 | 3 | 1 |
| Cycle | 0 | 47 | 47 |

| | |
|-------------------------------------|------------------------------------|
| Electric car charging points | 3 Active (23%) 10 Passive (77%) |
|-------------------------------------|------------------------------------|

| | |
|-------------------------------|---|
| Representation summary | <i>Adjoining neighbours were consulted by letter on 01.12.21 and 13.12.21. A Site Notice was displayed at the site on 03.12.21. A Press Advert was published on 15.12.21 in the News Shopper.</i> |
| Total number of responses | 36 |

| | |
|----------------------------|----|
| Number in support | 2 |
| Number of objections | 33 |
| Number of general comments | 1 |

| Section 106 Heads of Term | Amount | Agreed in Principle |
|---------------------------|------------|---------------------|
| Carbon offsetting | £59,195.00 | |
| Monitoring fee | £500 | |

SUMMARY OF KEY REASONS FOR RECOMMENDATION

- The principle to redevelop the site including the enhancement of library provision, loss of the public car park, and introduction of residential units is supported at this town centre location.
- The proposed redevelopment of the library would provide an enhanced provision of community service(s) and its design and appearance would respect that of the streetscene and area within which it lies.
- This application includes the provision of 26 residential dwellings which would represent a moderate contribution to the supply of housing within the Borough. This would include 14 social rented affordable units.
- The proposed layout of the dwellings would offer a good quality of internal amenity alongside a suitable provision of external amenity spaces and childrens play area, and would have an acceptable impact on the neighbouring residential amenities in terms of daylight/sunlight conditions, privacy and outlook.
- The proposal would provide sufficient and appropriately laid out car parking for disabled persons, bicycle and refuse/recycling storage.
- The development is considered acceptable from a sustainability, drainage, air quality and environmental perspective.
- The scale and design of the proposed flatted block (Block A) is considered to be out of keeping with the character and appearance of Croft Avenue within which it would lie.
- Having considered the benefits and harm arising from the proposal and in the absence of a 5-year housing land supply, it is considered that the planning permission should be granted as the presumption in favour of sustainable development is applied unless there are material considerations to suggest otherwise.
- Subject to the planning conditions it is considered that the benefits of the proposal would outweigh the impact arising from this proposal and planning permission should be granted.

1 LOCATION

- 1.1 The application site comprises West Wickham Library which is located on Glebe Way close to the junction with Station Road and a Council car park to the rear on Croft Avenue.



Fig.1 Existing site location plan.

- 1.2 The library site is a predominantly single storey building. The main element has a pitched roof with some space within the roof (although this is not visible from the outside). There is a single storey flat roofed element to the east elevation, with the southern elevation of this element comprising the main entrance, and a single storey flat roofed element to the west elevation which comprises self-contained public toilets (not linked to the library).





Fig.2 Photographs of the library site and surroundings from Glebe Way.

- 1.3 The carpark site is an open surface carpark of level tarmac hard surfacing of approximately 67 spaces which is accessed via Croft Avenue.
- 1.4 The carpark also provides access to No. 105A Station Road which does not form part of the application site. There is also a private carpark to the north-west of the library site which also does form part of the application site.





Fig.3 Photographs of the car park site and surroundings

- 1.5 The whole of the application site lies within the West Wickham Town Centre Area (TCA) which is a District Centre.
- 1.6 The library lies within a Secondary Retail Frontage which wraps around the corner of Glebe Way to the west and continues along Station Road. The properties along this part of Station Road comprise mainly two storey post war buildings with commercial at ground floor and mostly residential at first floor (some include accommodation at second floor level). These properties back on to a single service track which lies adjacent to the carpark site.
- 1.7 The wider area to the north and east is residential in nature and characterised mainly by detached and semi-detached two storey post war dwellings.
- 1.8 The public transport accessibility of the site is rated at 2 on a scale between 0 to 6b where 0 is worst and 6b is best.

2 PROPOSAL

- 2.1 Planning permission is sought for the refurbishment and extension to West Wickham library including a new cafe and community space; redevelopment of the car park site on Croft Avenue to provide a mews of 6 houses comprising 12 flats (6 x 1 bed and 6 x 3 bed) and a detached three storey block of flats comprising 14 units (3 x studio and 11 x 1 bed); with associated bike and bin stores, car parking, ancillary space and private and communal amenity space and alterations to the access road.



Fig.4 – Proposed Ground Floor Site Plan

- 2.1 The application involves two physically separate sites; library and public toilets, and public car park, producing an enhanced library provision and new residential development.
- 2.2 Library: The redevelopment of the existing library and attached vacant toilet block to create an additional 374sq.m. of new floorspace (a total of 863sq.m.) would involve;
 - Replacing the vacant toilet block with a new main entrance and café extension to include a mezzanine level for café seating;
 - Internal layout changes at ground floor to include a new Makerspace and children's library with soft landscaped outdoor space to the rear for children's library outdoor classroom;
 - New mezzanine level to provide additional library space;
 - Roof extension to provide a new community space, business hire lounge/workspaces and teenage and adult library space at first floor;
 - New hard and soft landscaping to corner of Glebe Way and Station Road to provide an urban public square.

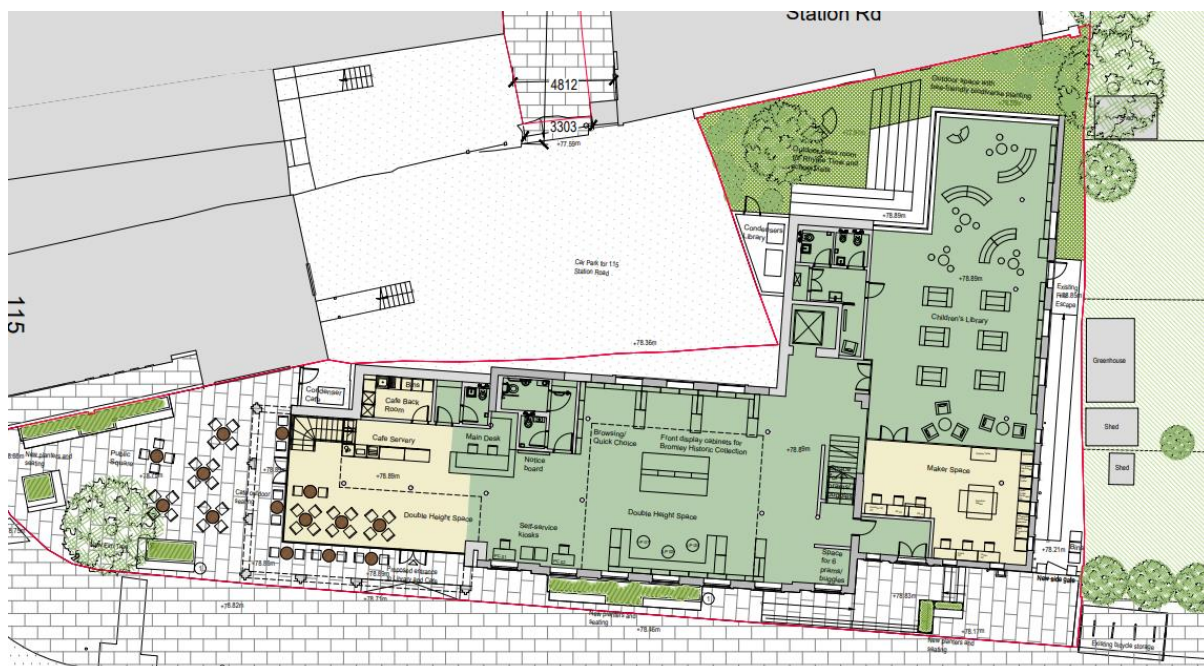


Fig.5 – Library Proposed First Floor Plan

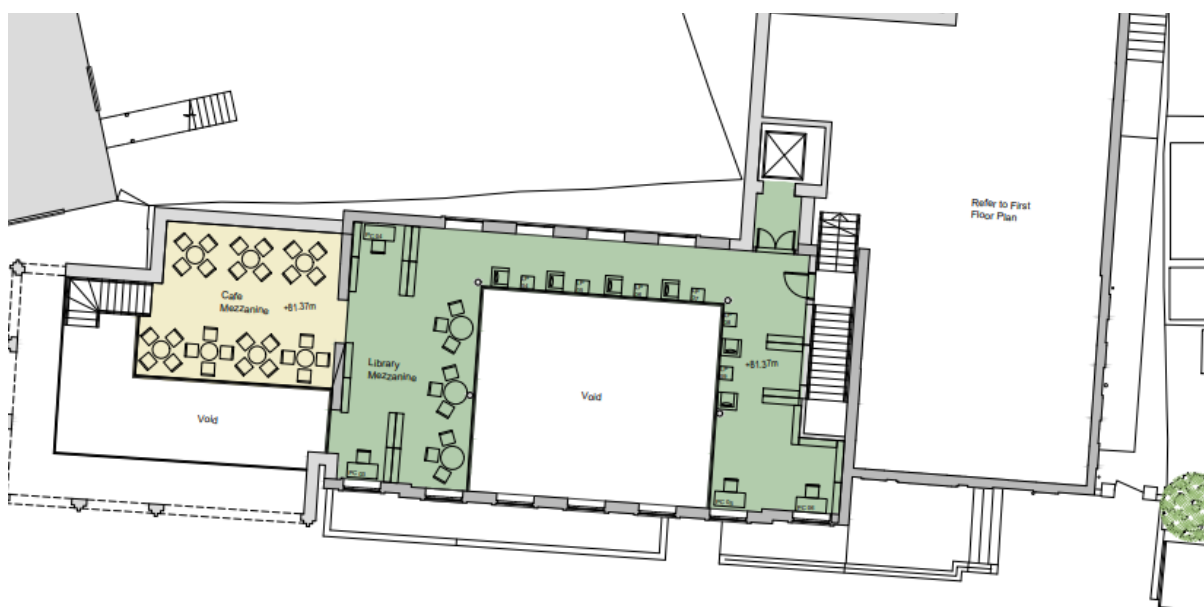


Fig.6 – Library Proposed Mezzanine Floor Plan

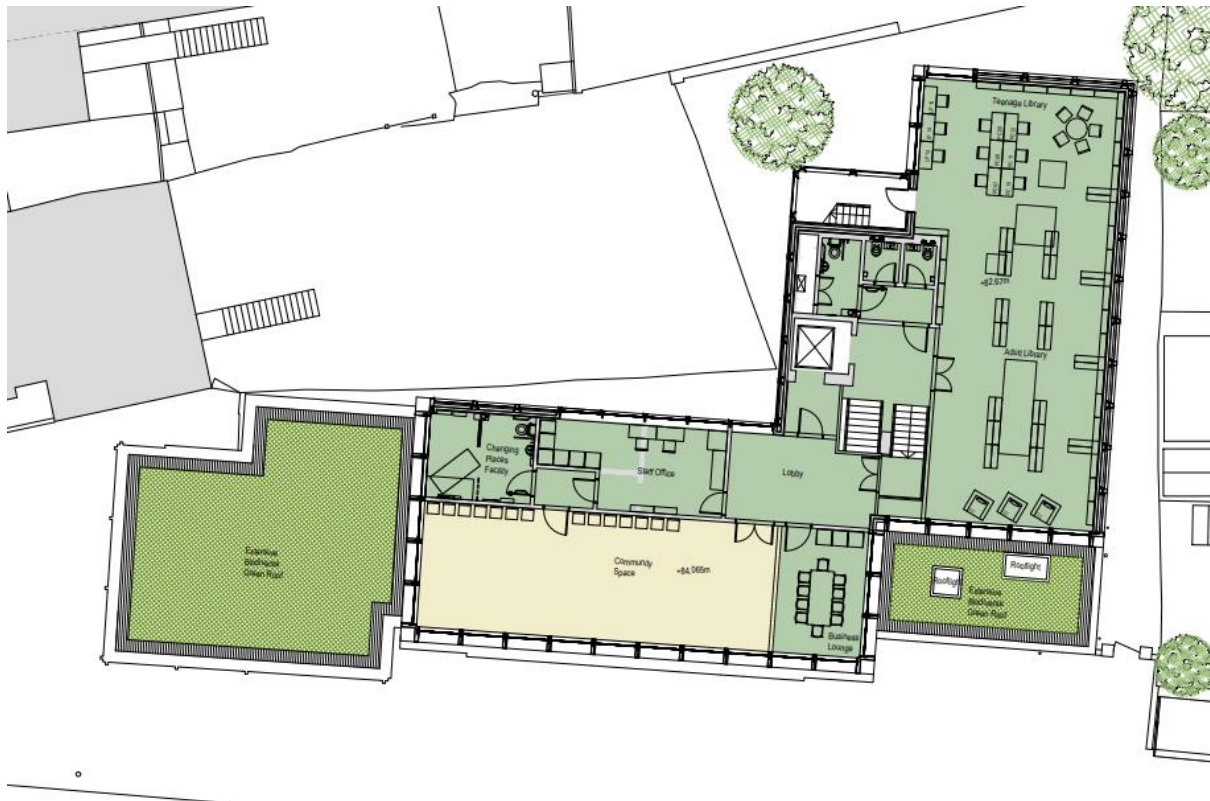


Fig.7 – Library Proposed Second Floor Plan

2.3 Residential: A new residential development comprising a total of 26 units is proposed on the site of the existing Council car park located to the rear of the library, accessed from Croft Avenue.

- Block A would comprise 1 x three storey block of 3 x studio and 11 x 1-bedroom affordable flats with balconies/terraces, Block A would front Croft Avenue.
- A row of six terraced mews maisonettes containing 12 x market units of 6 x 1-bedroom units at ground floor with rear garden areas and 6 x 3-bedroom units with roof terraces on the upper floors is proposed to the rear of Block A and accessed from Croft Avenue via the existing service access road located to the west of the site to the rear of the properties on Station Road.

- 2.4 A total of 13 car parking spaces are proposed – 5 car parking spaces located to the front of Block A which would include 2 designated disabled bays and would have direct access from Croft Avenue; and 8 car parking spaces located to the southern side of the Mews housing adjacent to No. 105A Station Road which would include 1 designated disabled bay and would be accessed via the existing service access road located to the west of the site to the rear of the properties on Station Road.
- 2.5 The application proposed to modify and formally adopt this existing service access road to the west of the site to the rear of the properties on Station Road.
- 2.6 Communal play area is proposed between Block A and the Mews housing.
- 2.7 A communal bin store is proposed to the front of Block A adjacent to the car parking spaces for the occupants of this flatted development. An internal communal bike store is proposed within the ground floor layout of Block A. The Mews housing would include separate bin and bike stores within each plot.

3 RELEVANT PLANNING HISTORY

- 3.1 There is no relevant planning history relating to the application site.
- 3.2 The following summarises the relevant planning history relating to No. 105A Station Road which lies between the library and carpark.
- Under ref: 00/03795/FULL1, planning permission was granted for a detached single storey building for use as health and beauty salon, with car parking space.
 - Under ref: 02/03220/RECON, approval was granted for the continued use as health and beauty salon without complying with condition 5 of permission 00/03795 granted for detached single storey building for use as health and beauty salon, to allow opening Tuesdays, Wednesdays and Thursdays until 8 p.m.
 - Under ref: 19/05336/FULL1, planning permission was granted for change of use application from Beauticians (Sui Generis) into a Nursery (Use Class D1). Erection of a part boundary fencing to front and side elevation and front gate to provide outdoor play space.

4 CONSULTATION SUMMARY

A) Statutory

4.1 Highways - No objection

- Existing site
 - The existing site currently comprises a public car park providing 64 parking spaces which is owned by the Council, with access taken from the north on Croft Avenue. A service road access forms the western section of the site running parallel to the car parking, which provides a route to the rear of the commercial properties that front Station Road. The service road is not part of Bromley's road network and is in a poor condition with no footways on either side.

- Parking Surveys and loss of existing car park
 - Daytime parking surveys were undertaken at four Council run car parks across West Wickham, which included the site. An assessment removing the Station Road car parking spaces was also undertaken. The assessments demonstrate that the loss of the 64 car parking spaces at the site can be accommodated by the alternative public car parks in West Wickham. Therefore, the loss of 64 on-site car parking spaces will not have a materially detrimental impact on parking availability within the town centre.
 - Overnight parking surveys were also undertaken in order to understand the existing parking demand in the vicinity of the site. This included roads within a 200m walking distance of the site, with the survey undertaken by an independent survey company using the Lambeth Methodology. This indicated an average occupancy of 58.3% over the two nights.
- Access
 - The existing access into the car park will be made redundant and reinstated as footway, with the main access to the site taken from the existing service road, which will be improved as part of the proposals.
 - The width of the access road ensures that two-way vehicle movement is feasible for most of its length, with sufficient space for two cars to wait at the access.
 - This access road requires Private Street Works (PSW) to be carried out under section 228 of the Highway Act 1980. This must be secured by a condition.
 - The access road provides an improved pedestrian route through the site and provides access to 8 off street parking spaces, whilst providing space for refuse and delivery vehicles to turn within the site.
 - A junction visibility assessment has been undertaken for the access the results of which are acceptable.
 - It is proposed for the access road to become adopted and it will therefore need be built to adoptable standards based on a shared surface as the access itself serves less than 25 dwellings.
- Car Parking
 - A total of 13 car parking spaces are proposed, 3 of which will be for disabled users. 5 will be accessed directly from Croft Avenue and the remaining 8 spaces accessed via the internal access road to the south of the row of maisonettes. A total of 3 spaces will be fitted with active electric chagrining facilities, with all remaining spaces provided with passive provision. The parking provision is acceptable.
 - The row of existing parking bays to the south of the library on Glebe Way will remain as existing, maintaining short-stay parking.
- Cycle parking
 - 43 cycle parking spaces are proposed, 41 of which will be long-stay space provided in sheltered and secure storage. Two external visitor spaces are proposed in the form of a Sheffield hoop. The 8 sheltered cycle parking spaces on Glebe Way will be retained.

- The level of cycle parking is satisfactory as it is in line with London Plan 2021 standards.
- Servicing and Refuse Collection
 - A communal refuse and recycling bin store will be located to the north of the block of flats which will be associated with the block, with individual bins located in front of the row of flats/maisonettes.
 - All servicing and refuse collection can take place on-site, with vehicles able to access/egress the site in forward gear, with the associated vehicle swept path analysis indicating the Bromley's refuse vehicle can service the proposed development. This ensures that bin dragging distances are minimised for the Waste collection operatives, whilst vehicles can also wait on Croft Avenue adjacent to the communal bin store.
- Deliveries
 - Deliveries will take space on-site with access from Croft Avenue. The width of the access road ensures that a car can still pass a delivery vehicle if loading/unloading.
 - Based on survey information contained with the TRICS database, residential developments generate around 8 or 9 delivery/collections per 100 units per day on average. Based on this, the development is likely to generate 2-3 additional delivery/collection trips per day, on average.
- Trip Generation
 - In order to assess the potential number of trips associated with the proposed residential use, the TRICS database for affordable flats has been utilised, located within Greater London only and of a similar size development to the proposals and states that the proposed residential use has the potential to generate 26 two-way person trips across the morning peak hour and 16 two-way trips across the evening peak hour.
 - The modal split for the proposed residential use has been applied to the trip generation assessment. There will be approximately 12 and 7 additional car trips in the AM and PM peak periods respectively. This equates to one additional vehicle every 5 – 9 minutes. The existing site comprises a public car park which generates more vehicle trips than the proposed development. As such, the proposal will provide a net-benefit on the local highway network.
- Public Transport
 - The effect on rail trips is expected to be minimal.
 - The level of impact on buses services and public transport services generally will be negligible and fall within daily fluctuations.
- Conclusion: No objection in principle subject to the following conditions; design work for access road (under section 228 Highway Act 1980), full details of refuse storage, full details of cycle parking, details of highway drainage, implementation of car parking, details of a Car Parking Management Plan, details of a Delivery and Servicing Plan, details of Construction Management Plan.

4.2 Transport for London (TfL) -

- Access
 - Access to the site via Station Road will be reinstated as footway and main access to the site will be via a service road from Croft Avenue.
 - TfL will need to be re-consulted if there is any alteration of the access onto Glebe Way (TLRN).
- Cycle Parking
 - 41 long stay and 10 short stay cycle parking spaces are proposed, including cycle parking spaces on Glebe Way. This is not in accordance with Policy T5 as 43 long stay and 15 short stay spaces are required.
 - Cycle parking provision is not in accordance with London Cycle Design Standards (LCDS) with key areas of non-compliance being:
 - Proposed short stay provision not shown on plans and it should be ensured that this provision is within 25m of the buildings it is proposed to serve.
 - 0.5m has been proposed between the two-tier racks which does not meet the minimum spacing requirements in the LCDS.
 - 2.7m has been provided between the two-tier stands at the wall. LCDS is clear that a minimum width of 2.5m beyond the lowered frame is required to allow cycle to be turned and loaded.
 - No wider access stands have been provided. LCDS requires a minimum of five per cent of stands to be wider access Sheffield stands which equates to three wider access stands.
 - No Sheffield stands at normal spacing have been proposed in the cycle stores. LCDS is clear that two-tier racks are inaccessible for a fair proportion of likely users, including being totally unsuitable for children's cycles or for larger cycles typically used by disabled residents, and being much more difficult to use for older or disabled residents with conventional cycles. A minimum of 20 per cent Sheffield stands at normal spacing is recommended.
 - The cycle parking for the maisonettes is via a shared garden which raises concerns over the personal security of users who could easily be followed into these stores or, given that this is only one door, pushed back in when they try to exit. The way to resolve these concerns is to provide access to cycle stores through the residential lobby of each building.
- Car parking
 - 13 car parking spaces are proposed, including three disabled persons' spaces which accords with Policy T6.
 - The car parking is for residential use and so in line with Policy T6.1 should be leased and not sold. Arrangements for assigning car parking spaces should be in a Car Parking Management Plan, secured through condition.
 - In line with Policy T6, disabled persons' parking should not be allocated to specific dwellings.

- It is welcomed that 20 per cent of spaces will have active vehicle charging points with passive provision for all remaining spaces, in line with Policy T6.
- Construction, Delivery and Servicing
 - In line with Vision Zero, delivery and servicing vehicles should enter and exit the site onto Croft Avenue in forward gear. A swept path analysis detailing this should be provided.
 - A full Delivery and Servicing Plan should be secured through condition in line with Policy T4 and TfL guidance.
 - A full Construction Logistics Plan is required though a condition in line with Policy T7.
- Travel Plan
 - No outline travel plan has been provided. A full travel plan should be secured through condition.

4.3 **Drainage (lead local flood authority) – No objection**

- Condition required regarding the submission of the detailed design measures in the submitted “Flood Risk & Sustainable Drainage Assessment” Report carried out by Parmarbrook Consultant dated 07/10/2021.

B) Local Groups

4.4 **Bromley RSPB Local Group** (addressed in paragraph 6.8.11).

- It is recommended that the installation of 26 integral swift nest bricks is a planning condition on any approval.

4.5 **West Wickham Residents’ Association** (addressed in paragraphs 6.1.8-6.1.19 and 6.7.17).

- The parking count suggests that overall, the lost spaces could be accommodated in the remaining three council car parks in West Wickham, two of those car parks are not within reasonable walking distance of the shops served by this car park
- Loss of car parking spaces and the provision of only 8 non-disabled parking spaces for the 26 residential units and their visitors is inadequate and will lead to further on-street parking

C) Local Residents

OBJECTION

4.6 **Loss of car park** (addressed in paragraphs 6.1.8 – 6.1.19).

- Impact on local businesses as people won’t be able to park
- Impact on those who use the GP surgery
- Should be encouraging shoppers back to the High Street, not removing car parking.
- Increased pressure for parking on surrounding roads – recent development on Glebe Way has already severely impacted parking and this development will further worsen the situation.

- The Highways department should extend the parking restriction on Croft Avenue (at its junction with Station Road) to include Sunday or if not introduce double yellow lines at this junction for road safety purposes.
- The car park is often full and statements that it is not fully used are wrong.
- The surveys were done just after covid restrictions were lifted and people still weren't going anywhere
- The survey does not fully take account of travel behaviour as most drivers who use the car park are likely making local journeys and are approaching from the north/northeast and they will likely decide its quicker to park on Croft Avenue/Ash Grove/Oak Grove rather than use a car park further away.
- Where will the visitors to the library park and is there disabled parking for library visitors
- This car park is open and considered safe at night whereas the car park at the other end of the high street is not
- Will cause more congestion and accidents along the high street as cars go to the other car parks
- The alterations to the library do not justify the loss of parking

4.7 Insufficient parking provision for new residential properties (addressed in paragraph 6.7.17).

- Only 13 spaces to be provided and most residents will have at least 2 cars and so will have to park on surrounding roads which are already congested
- No parking for visitors impacting on surrounding roads
- Transport Report existing levels of car ownership is based on the 2011 census and includes Inner London so the analysis of parking needed for this development is grossly understated
- The use of the TRICS database for assessing trip generation for affordable flats is inappropriate in West Wickham because the vast majority of such units are in Inner London so the use of such data is inappropriate
- The market for the news flats will be severely restricted unless each includes a parking space

4.8 Issues with the access road (addressed in paragraphs 6.7.9 – 6.7.14).

- The access road will be difficult for two-way traffic and it looks that deliveries and refuse collections will find it difficult to turn where the residents parking is located
- No provision has been made for deliveries for the rear of the commercial premises in Station Road and the private car park at the rear of 115 Station Road
- The sight lines for vehicles existing the access road may be inadequate as the plans do not show the recent side and rear extensions at 75 Station Road
- The private road that the council is going to take possession of is one which the freeholders and leaseholders paid for when they purchased their property which isn't acceptable and will likely decrease their value

- 4.9 Impact on amenities of neighbouring residential properties (addressed in Section 6.6).
- Overlooking to local gardens and loss of privacy
 - Loss of light
 - Overshadowing
 - Noise and potential caused by the playground being erected next to neighbouring residents' garden.
 - Dominant and overbearing
- 4.10 A formal independent Right to Light survey should be completed (addressed in paragraph 6.6.12).
- 4.11 Impact of the housing development on character of area (addressed in paragraphs 6.4.12 – 6.4.20).
- Overdevelopment
 - Density and height of development is out of character
 - Three storey flatted block next to 1930s houses is not in keeping
- 4.12 Impact of library development on character of area (addressed in paragraphs 6.4.6 – 6.4.11).
- The library proposal is out of character of typical modern ugly architectural design
 - The extension and first floor are out of keeping with the attractive existing building
- 4.13 Insufficient refuse storage (addressed in paragraphs 6.7.33 – 6.7.38).
- 4.14 Amount of cycle storage being provided (addressed in paragraphs 6.7.22 – 6.7.24).
- 4.15 Concerns about public open space area (addressed in paragraph 6.4.10)
- There is already a public square at the junction of Station Road with Glebe Way and if it is underused it is because it is not maintained to an attractive standard.
 - The junction at Glebe Way/Station Road/High Street/Wickham Court Road experiences traffic related noise and air pollution and the design proposal for the public open space do not adequately respond to this context (lacking vegetation and associated screening between the junction and proposed seating area).
 - The design for this space should be revised.
 - The proposed Elm tree should be a mature specimen and not a young sapling.
- 4.16 The gardens for the ground floor flats to the Mews housing are 6.8m deep which is in breach of the Council guidelines requiring a 10m depth of garden which will encourage more backland development (addressed in paragraphs 6.3.14 – 6.3.17).
- 4.17 Impact of construction on Croft Avenue (addressed in paragraph 6.7.32).

- 4.18 Insufficient information on how Bromley intends to support the new London Plan for sustainability (addressed in Section 6.9).
- 4.19 The consultation for this application was inadequate (addressed in paragraph 6.6.12).
- 4.20 Not good use of public money as it is believed the aim is to move the library to the redeveloped swimming baths site (addressed in paragraph 6.6.13).
- 4.21 Devaluation of existing properties (addressed in paragraph 6.6.13).
- 4.22 There is no proven demand for the café extension or public hall space (addressed in paragraph 6.6.13).
- 4.23 Light pollution (addressed in paragraph 6.4.25).
- 4.24 Current local infrastructure will need to be increased to handle the increase in local population (addressed in paragraph 6.12.9).

SUPPORT

- 4.25 Support upgrade of West Wickham Library facilities
- 4.26 Understand need for affordable housing
- 4.27 The National Libraries Director for GLL, the social enterprise which operates library services on behalf of Bromley Council, would like to support this development. There is a real demand for library services in West Wickham and the new library will provide significantly improved facilities for local residents. Existing customers will benefit from improved design in areas such as the children's library, as well as new facilities, such as the makerspace, an area full of new technology which will allow customers of all ages to develop their skills and creativity. The new building will also be more attractive to people who do not currently use the library, with well-designed spaces for community use, as well as the café.
- 4.28 If any late representations are received they will be reported verbally at the committee meeting.

5 POLICIES AND GUIDANCE

- 5.1 Section 70(2) of the Town and Country Planning Act 1990 (as amended) sets out that in considering and determining applications for planning permission the local planning authority must have regard to:-
 - (a) the provisions of the development plan, so far as material to the application,
 - (b) any local finance considerations, so far as material to the application, and
 - (c) any other material considerations.

- 5.2 Section 38 (6) of the Planning and Compulsory Purchase Act (2004) makes it clear that any determination under the planning acts must be made in accordance with the development plan unless material considerations indicate otherwise.
- 5.3 The London Plan 2021 is the most up-to-date Development Plan Document for the London Borough of Bromley, and therefore, in accordance with section 38(5) of the Planning and Compulsory Purchase Act 2004, *“if to any extent a policy contained in a development plan for an area conflicts with another policy in the development plan the conflict must be resolved in favour of the policy which is contained in the last document to become part of the development plan.*
- 5.4 The application falls to be determined in accordance with the following policies:-
- 5.5 **National Policy Framework (2021)**
- 5.6 National SPG - Technical housing standards – Nationally Described Space Standard (March 2015)
- 5.7 **The London Plan (2021)**
- GG1 Building strong and inclusive communities
 - GG2 Making the best use of land
 - GG3 Creating a healthy city
 - GG4 Delivering the homes Londoners need
 - GG5 Growing a good economy
 - GG6 Increasing efficiency and resilience
 - SD6 Town centres and high streets
 - SD7 Town centres: development principles & Development Plan Documents
 - D1 London’s form, character and capacity for growth
 - D2 Infrastructure requirements for sustainable densities
 - D3 Optimising site capacity through the design-led approach
 - D4 Delivering good design
 - D5 Inclusive design
 - D6 Housing quality and standards
 - D7 Accessible housing
 - D8 Public realm
 - D11 Safety, securing and resilience to emergency
 - D12 Fire safety
 - D13 Agent of Change
 - D14 Noise
 - H1 Increasing housing supply
 - H2 Small sites
 - H4 Delivery affordable housing
 - H5 Threshold approach to applications
 - H6 Affordable housing tenure
 - H7 Monitoring of affordable housing

- H10 Housing size mix
- HC1 Heritage conservation and growth
- S1 Developing London's social infrastructure
- S4 Play and informal recreation
- G1 Green Infrastructure
- G5 Urban greening
- G6 Biodiversity and access to nature
- G7 Trees and woodlands
- SI 1 Improving Air quality
- SI 2 Minimising greenhouse gas emissions
- SI 3 Energy infrastructure
- SI 8 Waste capacity and net waste self-sufficiency
- SI 12 Flood risk management
- SI 13 Sustainable drainage
- T2 Healthy Streets
- T3 Transport capacity, connectivity and safeguarding
- T4 Accessing and mitigating transport impacts
- T5 Cycling
- T6 Car parking
- T6.1 Residential parking
- T6.4 Hotel and leisure uses parking
- T6.5 Non-residential disabled persons parking
- T7 Deliveries, servicing and construction
- DF1 Delivery of the plan and planning obligations
- M1 Monitoring

The relevant London Plan SPGs are:

- Providing for Children and Young People's Play and Informal Recreation (2012)
- Accessible London: Achieving an Inclusive Environment (2014)
- Sustainable Design and Construction (2014)
- Shaping Neighbourhoods: Character and Context (2014)
- Control of Dust and Emissions During Construction and Demolition (2014)
- Housing (2016)
- Energy Assessment Guidance (2020)
- Homes for Londoners - Affordable Housing and Viability (2017)
- Homes for Londoners: Affordable Homes Programme 2021-2026 Funding Guidance (November 2020)

5.8 **Bromley Local Plan (2019)**

- 1 Housing Supply
- 2 Affordable Housing
- 4 Housing Design
- 8 Side Space
- 20 Community Facilities
- 21 Opportunities for Community Facilities

- 22 Social Infrastructure in New Developments
- 30 Parking
- 31 Relieving Congestion
- 32 Road Safety
- 33 Access to services for all
- 34 Highway Infrastructure Provision
- 37 General Design of Development
- 73 Development and Trees
- 77 Landscape Quality and Character
- 79 Biodiversity and Access to Nature
- 94 District Centres
- 113 Waste Management in New Development
- 115 Reducing Flood Risk
- 116 Sustainable Urban Drainage Systems
- 117 Water and Wastewater Infrastructure
- 118 Contaminated Land
- 119 Noise Pollution
- 120 Air Quality
- 122 Light Pollution
- 123 Sustainable Design and Construction
- 124 Carbon Reduction, Decentralised Energy Networks & Renewable Energy
- 125 Delivery and Implementation of the Local Plan

The relevant Bromley SPGs are:

- Planning Obligations (2010) and subsequent addendums
- Affordable Housing (2008) and subsequent addendums
- SPG1 General Design Principles
- SPG 2 Residential Design Guidance

6 ASSESSMENT

The main issues to be considered in respect of this application are:

- Principle of Development
- Housing
- Residential Standards
- Design, Scale, Layout and Landscaping
- Heritage
- Impact on Neighbouring Amenities
- Transport and Highways
- The Natural Environment
- Energy and Sustainability
- Drainage
- Technical Matters
- Planning Obligations and CIL

6.1 **Principle of Development - Acceptable**

Land Use (acceptable)

- 6.1.1 The application site lies within the District Town Centre of West Wickham. Policy SD6 of the London Plan encourages the promotion and enhancement of London's varied town centres including appropriate mixed-use or housing-led intensification to optimise residential growth potential, securing a high-quality environment and complementing local character and heritage asset. It also supports the enhancement of the provision of social infrastructure.
- 6.1.2 The application site lies between commercial/mixed-use properties on Station Road, High Street and Glebe Way and residential properties on Croft Avenue and Oak Grove. The principle of residential development in this location is therefore considered acceptable.

Proposed Library Redevelopment (acceptable)

- 6.1.3 Policy S1 Developing London's social infrastructure, supports social infrastructure and co-location which are particularly encouraged in town centres stating;

'...C Development proposals that provide high quality, inclusive social infrastructure that addresses a local or strategic need and supports service delivery strategies should be supported.

D Development proposals that seek to make best use of land, including the public-sector estate, should be encouraged and supported. This includes the co-location of different forms of social infrastructure and the rationalisation or sharing of facilities.

E New facilities should be easily accessible by public transport, cycling and walking and should be encouraged in high streets and town centres.'

- 6.1.4 Bromley Local Plan Policy 20 'Community Facilities' lends support to the principle of the proposed enhancement of the library, stating that:

'The Council will promote the quality of life and the health and wellbeing of those living and working in the Borough and engage with providers and agencies to ensure the provision, enhancement and retention of a wide range of appropriate social infrastructure, including facilities for health and education; recreation, sports and play facilities, places of worship and venues for cultural and social activities,...

- 6.1.5 Policy 21 of the Bromley Local Plan (Opportunities for Community Facilities) also supports *'the maximisation of opportunities for the enhancement or the creation of social infrastructure,'* specifically it supports community uses in District centres (clause b) and encourages *'the development of community "hubs" providing a range of social infrastructure on accessible existing community sites or in retail centres ...'* (clause c). Clause d also encourages *'the cultural and leisure use of the public realm'.*

6.1.6 The principle of the proposed redevelopment and enhancement of the existing library is therefore supported by Policies 20 and 21 of the Bromley Local Plan. However, these policies do also expect that proposals *'provide appropriate parking and should not adversely affect highway safety or the amenities of adjoining occupiers'* and requires that facilities ensure that they are easily accessible to all sections of the community, through the principles of inclusive design. These matters will be considered in the relevant sections of this report below.

Loss of Public Car Park (acceptable)

6.1.7 The application also involves a new residential development on a public car park located to the rear of the library, off Croft Avenue. This public car park provides 64 parking spaces and serves West Wickham District Centre which consists predominantly of units along the High Street and Glebe Way.

6.1.8 It is acknowledged that a number of the concerns raised locally relate to the loss of the car park as well as the impact of this on the surrounding roads.

6.1.9 There are no planning policy requirements which require the justification of the loss of the public car park. However, the applicant has provided a Transport Statement with the application, which includes parking survey's (prepared by Caneparo Associates dated October 2021) and an Economic Assessment (prepared by Volterra Partners dated October 2021) to demonstrate how the displaced demand can be accommodated by other public car parks within the town centre.

6.1.10 As part of the submission, Daytime parking surveys were undertaken at four Council run car parks across West Wickham, which included the application site, on Friday 2nd July 2021 and Saturday 3rd July 2021 between 09:00 – 19:00.

6.1.11 The four car parks and their capacity are outlined below:

- Station Road Car Park: 64 spaces (62 standard + 2 disabled) – application site;
- West Wickham High Street, Car Park: 115 spaces (113 standard + 2 disabled);
- Ravenswood Avenue Car Park: 143 spaces (139 standard + 4 disabled);
- West Wickham Pools Car Park: 54 spaces (52 standard + 2 disabled).

6.1.12 Table 2.1 within the accompanying Transport Statement (shown in Fig.4 below) reveals that the peak hour across the two surveyed days was 13:00 on the Friday when 255 cars were parked and 121 spaces were still available, which represents a 68% parking occupancy. On average across the surveyed time period there were 216 available spaces on the Friday and 224 spaces available on the Saturday.

6.1.13 The raw traffic survey data also reveals that peak parking demand of the disabled spaces was at 60% on both Friday and Saturday, with 6 cars parked out of a total of 10 spaces.

| Period | Friday 2 nd July 2021 | | | Saturday 3 rd July 2021 | | |
|----------------|----------------------------------|------------|---------------|------------------------------------|------------|---------------|
| | Cars Parked | Spaces | Occupancy (%) | Cars Parked | Spaces | Occupancy (%) |
| 09:00 | 106 | 270 | 28% | 73 | 303 | 19% |
| 10:00 | 169 | 207 | 45% | 162 | 214 | 43% |
| 11:00 | 222 | 154 | 59% | 247 | 129 | 66% |
| 12:00 | 236 | 140 | 63% | 243 | 133 | 65% |
| 13:00 | 255 | 121 | 68% | 224 | 152 | 60% |
| 14:00 | 236 | 140 | 63% | 211 | 165 | 56% |
| 15:00 | 187 | 189 | 50% | 192 | 184 | 51% |
| 16:00 | 113 | 263 | 30% | 140 | 236 | 37% |
| 17:00 | 98 | 278 | 26% | 81 | 295 | 22% |
| 18:00 | 61 | 315 | 16% | 52 | 324 | 14% |
| 19:00 | 79 | 297 | 21% | 46 | 330 | 12% |
| Average | 160 | 216 | 43% | 152 | 224 | 40% |

Fig.8 – Daytime Parking Survey Results (376 Spaces in Total)
(Table 2.1 within accompanying Transport Statement)

6.1.14 Overnight Parking Surveys of the Local Area were also undertaken on Tuesday 29th June 2021 (04:00) and Wednesday 30th June 2021 (05:00) in order to understand the existing parking demand in the vicinity of the site. This included roads within a 200m walking distance of the site, with the survey undertaken by an independent survey company using the Lambeth Methodology.

| Period | Cars Parked | Observed Spaces | Occupancy (%) |
|--|-------------|-----------------|---------------|
| Tuesday 29 th June 2021 (04:00) | 57 | 37 | 60.6% |
| Wednesday 30 th June 2021 (05:00) | 52 | 41 | 55.9% |
| Average | 55 | 39 | 58.3% |

Fig.9 Overnight Parking Survey Results
(Table 2.2 within accompanying Transport Statement)

6.1.15 The removal of 64 parking spaces from the site will result in a total of 312 spaces still being available across the 3 remaining car parks (West Wickham High Street, Ravenswood Avenue and West Wickham Pools).

| Period | Friday 2 nd July 2021 | | | Saturday 3 rd July 2021 | | |
|----------------|----------------------------------|------------|---------------|------------------------------------|------------|---------------|
| | Cars Parked | Spaces | Occupancy (%) | Cars Parked | Spaces | Occupancy (%) |
| 09:00 | 106 | 206 | 34% | 73 | 239 | 23% |
| 10:00 | 169 | 143 | 54% | 162 | 150 | 52% |
| 11:00 | 222 | 90 | 71% | 247 | 65 | 79% |
| 12:00 | 236 | 76 | 76% | 243 | 69 | 78% |
| 13:00 | 255 | 57 | 82% | 224 | 88 | 72% |
| 14:00 | 236 | 76 | 76% | 211 | 101 | 68% |
| 15:00 | 187 | 125 | 60% | 192 | 120 | 62% |
| 16:00 | 113 | 199 | 36% | 140 | 172 | 45% |
| 17:00 | 98 | 214 | 31% | 81 | 231 | 26% |
| 18:00 | 61 | 251 | 20% | 52 | 260 | 17% |
| 19:00 | 79 | 233 | 25% | 46 | 266 | 15% |
| Average | 160 | 152 | 51% | 152 | 160 | 49% |

Fig.10 – Assessment of Removing Station Road Car Park (312 Remaining Spaces)
(Table 5.3 within accompanying Transport Statement)

6.1.16 The above table indicates peak parking occupancy would be 82% at 13:00 on the Friday, with 57 spaces still available. On the Saturday, peak parking demand would be 79% at 11:00 with 65 spaces still available, with a reduced demand at all other times, therefore demonstrating that there is sufficient capacity across the remaining 3 car parks to accommodate expected demand.

6.1.17 In addition, it is acknowledged that Ravenswood Avenue car park is approximately only 110m west of the site, equivalent to a 1–2-minute walk and as such with only a slight increase in walking distance, it is likely to be used by people who currently use Station Road car park.

6.1.18 The Council's Highways Officer has reviewed the submitted Transport Statement and parking surveys and is satisfied that this sufficiently demonstrates that there are alternative public car parks in West Wickham which can accommodate the displaced demand and thus justify the loss of the public car park.

6.1.19 Whilst the principle to redevelop the site to provide enhanced library provision and new residential units is supported, this is subject to the planning considerations and requirements in the Development Plan. The overall planning balance of the proposal, having regard to the presumption in favour of sustainable development, is set out in the following sections of this report.

6.2 Housing – Acceptable

Housing Supply

- 6.2.1 The current position in respect of Bromley's Housing Trajectory, including the Five Year Housing Land Supply (FYHLS), was agreed at Development Control Committee on 2nd November 2021. The current position is that the FYHLS (covering the period 2021/22 to 2025/26) is 3,245 units, or 3.99 years supply. This is acknowledged as a significant undersupply and for the purposes of assessing relevant planning applications means that the presumption in favour of sustainable development will apply (paragraph 11(d) of the NPPF 2021).
- 6.2.2 The proposal would provide 26 residential units, which would represent a significant contribution to the Council's housing supply, in accordance with Policy 1 of the Local Plan.

Housing mix (acceptable)

- 6.2.3 Policy H10 of the London Plan states that schemes should generally consist of a range of unit sizes and regard should be had to local evidence of need.
- 6.2.4 Local Plan Policy 1 Supporting Text (paras 2.1.17 and 2.1.18) highlight findings from the 2014 Strategic Housing Market Assessment (SHMA) that the highest level of need across tenures within the Borough up to 2031 is for one-bedroom units (53%) followed by 2-bedroom (21%) and 3-bedroom (20%) units. Larger development proposals (i.e. of 5+ units) should provide for a mix of unit sizes and considered on a case by case basis.
- 6.2.5 The application proposes a mix of unit sizes (across all tenures) as follows;

| 1B1 | 1B2 | 1B1 WCA | 1B2 WCA | 2B3 | 2B4 | | 3B4 | UNITS |
|-----|-----|---------|---------|-----|-----|--|-----|-------|
| 3 | 14 | 0 | 3 | 0 | 0 | | 6 | 26 |
| 12% | 54% | 0% | 12% | 0% | 0% | | 23% | 100% |

Fig. 11 – Unit size mix

- 6.2.6 The LBB Housing Register December 2019 indicates a significant need for 1 and 2 bedroom social-rented/affordable-rented units. The 1-bed social rented units are considered to be acceptable as they would meet the current need. Whilst the number of studio units is less desirable, the overall mix is, on balance, considered to be acceptable.

Affordable Housing (acceptable)

- 6.2.7 Policy H4 of the London Plan (Delivering Affordable Housing) states that;

“The strategic target is for 50 per cent of all new homes delivered across London to be genuinely affordable. Specific measures to achieve this aim include:

...

4) public sector land delivering at least 50 per cent affordable housing on each site and public sector landowners with agreements with the Mayor delivering at least 50 per cent affordable housing across their portfolio.

- 6.2.8 Policy H5 C of the London Plan, states that in order to follow the Fast Track Route of the threshold approach, applications must meet all the following criteria:

*“1) meet or exceed the relevant threshold level of affordable housing on site without public subsidy
 2) be consistent with the relevant tenure split (see Policy H6 Affordable housing tenure)
 3) meet other relevant policy requirements and obligations to the satisfaction of the borough and the Mayor where relevant
 4) demonstrate that they have taken account of the strategic 50 per cent target in Policy H4 Delivering affordable housing and have sought grant to increase the level of affordable housing.”*

6.2.9 Policy H6 Affordable Housing Tenure Clause A specifies that the following affordable split should be applied to residential developments:

1. Minimum 30% low cost rental (London affordable / social rent),
2. Minimum 30% intermediate (London Living Rent / London Shared Ownership)
3. the remaining 40% to be decided by the borough as either low cost rent or intermediate units.

6.2.10 The application provides 26 homes of which 14 (53.8%) are Social Rented and 12 (46.2%) are Private market as set out in the table below.

| | Unit Type | SR | MV | Total | Total Number of Habitable Rooms |
|--------------|-----------------|-----------|-----------|-----------|---------------------------------|
| | 1B1p studio | 5 | 0 | 5 | 5 |
| | 1B2p flat | 9 | 6 | 15 | 30 |
| | 3B4p maisonette | 0 | 6 | 6 | 24 |
| Total | | 14 | 12 | 26 | 59 |

Fig.12 – Tenure and Unit mix

6.2.11 Policy H6 Clause B allows proposals to follow the fast track route where low-cost-rented housing (including social rented) is proposed instead of intermediate housing provided the threshold level is reached.

6.2.12 The application exceeds the London Plan 50% requirement and the Bromley Local Plan 35% requirement. 100% of the proposed affordable housing is social rented and therefore H6 Clause B is met.

6.2.13 The applicant has confirmed that they have sought the maximum amount of grant for this scheme given the constraints of the site and the overall objectives of the development.

6.2.14 Having regard to the above, the requirements of Policy H5 C are considered to be met and qualifies for the fast track route. An early stage review mechanism in accordance with the fast track route will be conditioned as part of the scheme.

6.3 Residential Standards – Acceptable

- 6.3.1 The NPPF para 130 sets an expectation that new development will be designed to create places that amongst other things have a 'high standard' of amenity for existing and future users.

Internal Amenity: Size, Privacy, Outlook and Daylighting (acceptable)

- 6.3.2 The space standards for residential development are set out in Table 3.1 of the London Plan and the Government published '*Technical housing standards - nationally described space standard*'. This is supported by Policy D6 of the London Plan, the Mayor's 'Housing' SPG 2016 and Bromley Local Plan Policies 4 and Policy 37.
- 6.3.3 The submitted floor plans contain details of furniture and layouts for each of the proposed residential units. The plans also indicate the number of occupants that would be accommodated, and the application is accompanied by a schedule of accommodation which demonstrates that all the proposed units would meet or exceed the required GIA for their size and occupation.
- 6.3.4 The section drawings indicate that the floor to ceiling heights for all the residential units within both the mews housing and flatted block would accord with the minimum requirement of 2.5m for at least 75 per cent of the GIA.
- 6.3.5 The proposed layout for the residential development has been designed to minimise mutual overlooking both within the site and towards neighbouring residential properties whilst still maintaining a good degree of outlook for prospective occupants.
- 6.3.6 The applicant has indicated that the proposed residential development has been designed to maximise best orientation, minimise overshadowing to neighbours and its own units.
- 6.3.7 The units in the Mews houses are East/West facing and are dual aspect. None of the flats would be overshadowed by new or existing buildings, and while open kitchen/living rooms go up in depth to a maximum of 7m, the living spaces are kept to 3-4m depth and have generous fenestration and/or are dual aspect.
- 6.3.8 The majority of the units within Block A are dual aspect, with only those located the rear with south facing windows being single aspect only. The applicant has confirmed that there would be minimal overshadowing of 2 windows located on the shorter East facing run of the wall on the ground and first floor of the block of flats. These windows serve mainly the kitchen area in an open plan kitchen/living room that is also provided with another larger window serving the living space.
- 6.3.9 Open kitchen/living rooms are generally kept under 5m room depth with the living spaces at 3-4m depth. These spaces are then provided with either dual aspect or generous fenestration.

Wheelchair unit and inclusive living environment (acceptable)

- 6.3.10 In accordance with Policy D7 of the London Plan and Local Plan Policy 4, 90% of new housing should meet Building Regulation Requirement M4(2) 'accessible and adaptable dwellings' and 10% of the new housing should meet Requirement M4(3) 'wheelchair user dwellings', i.e. is designed to be wheelchair accessible, or easily adaptable for residents who are wheelchair users.
- 6.3.11 The application proposes three accessible wheelchair units within the new residential Block A which will be located on the ground, first and second floors of this building. These units will be part of the affordable housing provision. These units have been reviewed by the Council's Occupational Therapist and whilst some concerns have been raised regarding the ability of the units to comply with Building Regulations M4(3) at present, it is considered that subject to changes to the internal layout the units would be suitable for occupation by a wheelchair user. It is proposed that compliance with the Building Regulations M4(3) standard for the three affordable rented units is secured by condition.
- 6.3.12 A lift is also proposed within the flatted Block A which would serve the upper floor units of this block.

Private outdoor space (acceptable)

- 6.3.13 Policy D6 of the London Plan and Standards 26 and 27 of the Mayor's London Housing SPG requires a minimum of 5sq.m private outdoor space to be provided for a 1 to 2 person dwelling and an extra 1sq.m to be provided for each additional occupant, and it must achieve a minimum depth and width of 1.5m.
- 6.3.14 The Mews housing would provide rear gardens of 40sqm. for the 1-bedroom ground floor flats and second floor front roof terraces of between 12sqm. and 14sqm. for the 3-bedroom upper floor units. The studio and 1-bedroom units within the flatted block would also include balconies/terraces of a minimum of 5sqm. All the balconies and terraces would also meet the minimum 1.5m width requirement.
- 6.3.15 Whilst it is regrettable that the 3-bedroom units within the Mews housing would have roof terraces rather than gardens, these spaces would still comply with the minimum standards for private amenity areas and would provide useable private outdoor space. There would also be a communal play area on site for any child occupants of these units.
- 6.3.16 The proposed private amenity space would comply with the relevant policies as outlined above.

Child play space (acceptable)

- 6.3.17 Policy S4 B of the London Plan refers to development proposals for schemes that are likely to be used by children and young people and states that for

residential developments at least 10 sqm of playspace should be provided per child, with criteria setting the nature of the playspace.

6.3.18 The proposal indicates 85sqm of playspace for children 0-17yrs to the rear of the proposed block of flats.

6.3.19 Whilst this exceeds the 53.9sqm that the GLA Child Yield calculator indicates is necessary for the scheme as detailed, its location in immediate close proximity to the ground floor wheelchair unit is potentially unneighbourly. A detailed design of the play area to ensure it is appropriate in terms of its use would also be required. Therefore, a condition will be placed on any approval to ensure the space is laid out in a way to adequately protect the amenities of all neighbouring properties and to secure the best practical use of the space.

6.4 Design, Scale, Layout and Landscaping

6.4.1 Paragraph 126 of the NPPF (2021) states that the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities.

6.4.2 London Plan and Bromley Local Plan policies further reinforce the principles of the NPPF setting out a clear rationale for high quality design.

6.4.3 Policy D3 of the London Plan relates to 'Optimising site capacity through the design-led approach' and states that all development must make the best use of land by following a design-led approach that optimises the capacity of sites. Form and layout should enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape. The quality and character shall respond to the existing character of a place by identifying the special and valued features and characteristics that are unique to the locality and respect, enhance and utilise the heritage assets and architectural features that contribute towards the local character.

6.4.4 Policy D4 of the London Plan outlines the various methods of scrutiny that assessments of design should be based on depending on the level/amount of the development proposed for a site.

6.4.5 Policy 4 of the Bromley Local Plan seeks to ensure that all new housing developments achieve a high standard of design and layout whilst enhancing the quality of Local Places, and Policy 37 of the Bromley Local Plan requires a high standard of design in all new development, and states that the scale and form of new residential development should be in keeping with the surrounding area.

Library - Acceptable

Layout

- 6.4.6 The application proposes to refurbish and extend the existing library building to provide additional facilities for the benefit of the local community. The main entrance would be relocated and a café function would be introduced with a colonnade to activate the street frontage and extend the public realm. The architectural approach is informed by local historical analysis which is also supported.

Scale & Massing

- 6.4.7 The scale and massing of the proposed library extension is considered to be acceptable and responds appropriately to the immediate and surrounding context. The setting back of the roof extension would also help the development respond to the surrounding heritage assets. It is also important that the contemporary roof extension appears subservient to the original building so as not to appear overly prominent or detract from its heritage value.



Fig.13 – Library Proposed South Elevation (facing Glebe Way)



Fig.14 – Library 3-D visual from Design and Access Statement

Architecture

- 6.4.8 The proportions of the extensions and their relationship with the existing building have been well considered. The simple layering of elements alongside a clear narrative has ensured that the ground floor café extension reads as a



Fig.16 – Proposed outdoor classroom area to rear of library

Summary

- 6.4.11 The scale and design of the proposed library extensions respond appropriately to the existing building and the surrounding context. The extension and activation of the public realm and the provision of additional community facilities is supported.

Housing Scheme – Mews Housing - Acceptable

Layout

- 6.4.12 The shared surface approach to the Mews development is accepted given the constraints of the site.
- 6.4.13 There are some concerns from an urban design perspective in relation to the blank flank wall at ground level on the southern end of the mews terrace adjacent to the car parking area. However, the first floor windows and the degree of overlooking provided by residents of Peacock House is accepted.

Scale & Massing

- 6.4.14 The 3 storey height of the mews terrace set 6.8 metres back from the rear gardens of existing properties is broadly acceptable. The second floor level would be partially within the pitched roof space which would help to reduce its bulk at its uppermost storey.

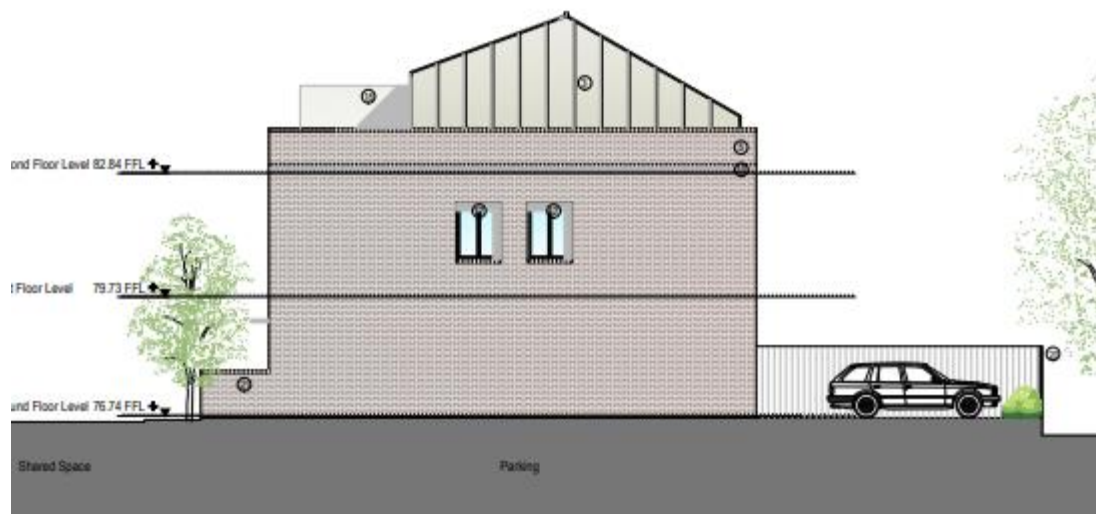


Fig.17 – Mews Housing Proposed Southern Side Elevation



Fig.18 – Mews Housing Proposed Visual of Front (West) Elevation

Architecture

6.4.15 The architectural approach to create a contemporary style but one which references the materiality and detailing of the surrounding local context is broadly supported. The use of a high-quality brick finish with detailing to add articulation and interest alongside suitably robust zinc roof cladding is welcomed. There is some concern with regard to the proposed the introduction of several contrasting brick types as indicated by the submitted Design and Access Statement, as creating a unified architectural language and the appearance of a 'family of buildings' with a clear identity is considered to be best achieved with a limited palette and the simple layering of elements. However, the external materials can be secured by condition on any approval.

Summary

6.4.16 The site lends itself to a mews style development given the constraints of the plot and the extent to which the design team have been architecturally inventive in order to seek to mitigate the impact on existing neighbouring residents is considered to be acceptable.

Housing scheme – Block A (flatted block) - Unacceptable

Scale and Massing (unacceptable)

- 6.4.17 The flatted urban block would introduce a typology that is out of keeping with the immediate traditional suburban character of Croft Avenue which is characterised by 2 storey houses and bungalow dwellings. However, its relationship with the closest neighbouring properties is key and in this regard the stepped massing would help to mitigate the step change in building heights between properties in Croft Avenue and those fronting the High Street. The opportunity to ‘close the urban block’ and reinstate a frontage to the southern side of Croft Avenue is also acknowledged.
- 6.4.18 Notwithstanding this, the scale of the block would nevertheless appear as a prominent addition to the streetscene given its large footprint and bulkier appearance which would be out of keeping with the character of Croft Avenue within which it lies.



Fig.19 – Apartment Housing Block A Proposed Front Elevation

Architecture (acceptable)

- 6.4.19 Similarly to the mews housing, the architectural approach references the materiality and detailing of the surrounding local context, which is broadly supported but ensuring high quality materials will be key and will help to mitigate the change in typology/step change in scale in Croft Avenue. As such, a condition requiring details of the external materials, including physical brick samples, to be submitted prior to any above ground works is considered necessary and reasonable on any approval.



Fig.20 – Proposed Visual of Apartment Housing Block A

Summary

6.4.20 The scale and design of 'Block A' represents a departure from the traditional character and appearance of Croft Avenue and the building typology, footprint, bulk and mass would appear visually prominent within the streetscene. However, it is noted that the architectural approach does reference the materiality and detailing of the surrounding local context and appropriate quality materials could be secured by condition.

Landscaping – Acceptable

6.4.21 Given the existing condition of the car park site, the opportunity to introduce urban greening in the form of additional tree planting, hedge planting and green roofs to enhance visual amenity and contribute to the scheme's Urban Greening Factor is welcomed.

6.4.22 There is concern that the communal playspace may result in a closed-off detached feel. However, through the introduction of seating and landscape interventions, this space could feel more integrated and it is accepted that this could be dealt with via a condition on any approval.

6.4.23 The surface treatment of 'left-over spaces' which would require maintenance and management, i.e. the areas labelled as 'flower perennial planting' fronting 'Block A' and the car parking area to the south, and similarly, the 'species rich grassland' area in the southeast corner would need to be carefully considered. The relationship between the gravel path fronting the eastern site boundary, the grassland area, and the perennial planting would also require further detail. These hard and soft landscaping details can be secured by way of an appropriate condition on any approval to ensure that they are acceptable.

6.4.24 Full details of the proposed boundary treatments (heights and appearance) have not been provided. However, these can also be secured by way of a suitable condition on any approval.

- 6.4.25 A condition requiring further details of lighting for the access road, car parking area and communal areas should also be secured by way of a condition to ensure this is suitable for users of the site and would not unduly impact the amenities of neighbouring residential properties.

Fire safety - Acceptable

- 6.4.26 The matter of fire safety compliance is covered by Part B of the Building Regulations. However, to ensure that development proposals achieve highest standards of fire safety, reducing risk to life, minimising the risk of fire spread, and providing suitable and convenient means of escape which all building users can have confidence in, applicants should consider issues of fire safety before building control application stage, taking into account the diversity of and likely behaviour of the population as a whole (London Plan Policy D12).

- 6.1.1 The supporting Fire Statement prepared by Lawrence Webster Forrest (dated March 2022) meets the requirements of Policy D12. Compliance to the fire statement will be conditioned however, compliance with the Building Regulations will still be required at the appropriate stage of the development.

Secured by Design - Acceptable

- 6.4.27 Supporting paragraph 3.3.14 of Policy D3 of the London Plan states development should reduce opportunities for anti-social behaviour, criminal activities, and terrorism, and contribute to a sense of safety without being overbearing or intimidating. Developments should ensure good natural surveillance, clear sight lines, appropriate lighting, logical and well-used routes and a lack of potential hiding places. This approach is supported by Policy D11 of the London Plan (Safety, security and resilience to emergency) and Bromley Local Plan Policy 37 (General Design of Development).

- 6.4.28 Appendix B of the supporting Design and Access Statement makes reference to a pre-application meeting on 02/06/2021 with Bromley's Designing Out Crime Officer (DOCO) and outlines the proposed standards. However, the DOCO has advised that the standards quoted are quite vague. Therefore, whilst they consider that the development can achieve Secured by Design, an appropriate two-part condition should be included on any approval requiring the principles and physical security requirements to be dealt with pre-commencement and the Secured by design accreditation achieved prior to occupation.

6.5 Heritage – Acceptable

- 6.5.1 The application site is located just to the east of The Swan Public House which is a Grade II Listed Building sited on the corner of Station Road and the High Street. No.'s 1 to 11 High Street, West Wickham, which lie to the south-west of the library, are also locally listed. These buildings are situated at the same junction with Glebe Way/Station Road/High Street as the library building. Therefore, due to its location, any development to the existing library building

would be expected to preserve or enhance the setting of these heritage assets in accordance with Policy HC1 of the London Plan.

- 6.5.2 The Council's Conservation Officer has advised that the scale and massing of the proposed library extensions would respond appropriately to these surrounding heritage assets. In particular, the setting back of the roof extension would help ensure that it appears subservient to the original building so as not to appear overly prominent or detract from its own heritage value or the setting of the nearby Listed Building and Locally Listed Buildings.
- 6.5.3 The proportions of the extensions and their relationship with the existing building and its appearance in relation to the nearby heritage assets are also considered to be acceptable. The ground floor café extension would read as a contemporary addition whilst remaining sympathetic to the original building and setting of the nearby heritage assets through its materiality and detailing. The patterned decorative metal screens referencing local context are considered to add another layer of interest which will compliment and lift the quality of the scheme. The quality of the brick finish would be very important and this can be secured by way of a condition on any approval.
- 6.5.4 The housing element of the proposed development is set behind the main frontage of the library building and further away from the heritage assets and as such is not considered to result in any impact to their setting.

6.6 Impact on Neighbouring Amenity – Acceptable

- 6.6.1 Policy 37 (e) of the Bromley Local Plan seeks to protect existing residential occupiers from inappropriate development. Issues to consider are the impact of a development proposal upon neighbouring properties by way of overshadowing, loss of light, overbearing impact, overlooking, loss of privacy and general noise and disturbance.

Privacy/Outlook (acceptable)

- 6.6.2 The proposed library extension would increase the height and bulk of the building when viewed from the rear of the residential properties located immediately to the east on Oak Grove. However, the extension would be sited a minimum of approximately 19m from the rear windows of these neighbouring dwellings which would help to reduce the impact on outlook to these residential properties.
- 6.6.3 The library extension has also been designed so that the eastern wing extension which lies closest to the rear boundaries of the residential properties on Oak Grove would not include any first floor east facing windows. The first floor east facing windows to the main roof extension of the library would be located at least 10m from the rear boundaries of these properties on Oak Grove with further separation provided by the residential gardens of these dwellings. Therefore, there would be no adverse loss of privacy.



Fig.21 – Library Eastern Side Elevation facing rear of properties on Oak Grove

- 6.6.4 The proposed Mews Housing would have their own individual gardens of 6.8m which would abut the rear boundaries of the gardens of the properties within Oak Grove. These properties on Oak Grove have rear gardens of between approximately 15.8m and 18.6m, meaning that a distance of at least 22m would be provided between the rear windows within the Mews Housing and the rear windows of these neighbouring properties to the east. In addition, the windows at second floor level would be rooflights with the main windows to the second floor accommodation being located within the western elevation leading onto the proposed external terrace on this side.
- 6.6.5 There are residential properties on Station Road to the west of the application site with rear windows which face the application site. However, the siting of the Mews Housing has been designed so that the communal car parking area and communal playspace would be located immediately to the rear of the closest residential properties on Station Road (No. 87/89 and Peacock House) helping to limit the impact on both outlook and privacy to these neighbouring residents. A greater degree of separation would be provided to the rear windows of the other first floor residential properties above the commercial premises of Station Road due to their existing location.

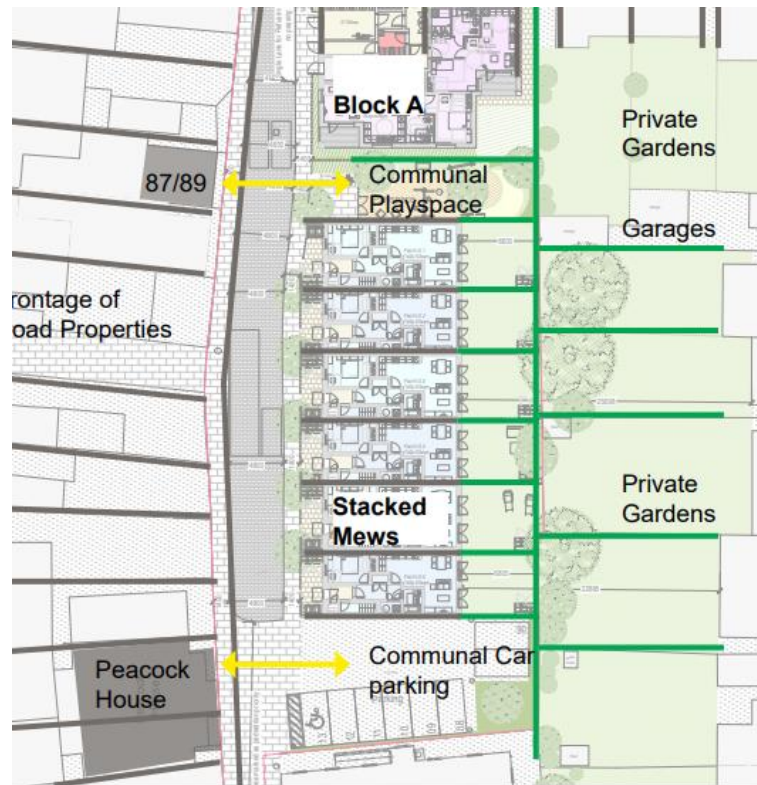


Fig.22 – Proposed Housing Site Layout

- 6.6.6 The proposed apartment housing, Block A, would have a staggered front and rear elevation helping to limit the visual impact when viewed from the front and rear windows of the immediately adjoining property at No. 2 Croft Avenue. There are windows located on the western elevation of this existing dwelling at No. 2 facing the application site; however, these are all obscure glazed and thus are unlikely to serve habitable rooms. This obscure glazing would also help limit any overlooking or loss of privacy towards No. 2.
- 6.6.7 The balconies to the rear would also be partially enclosed on the eastern side to prevent any adverse opportunities for overlooking towards the rear windows of No. 2.

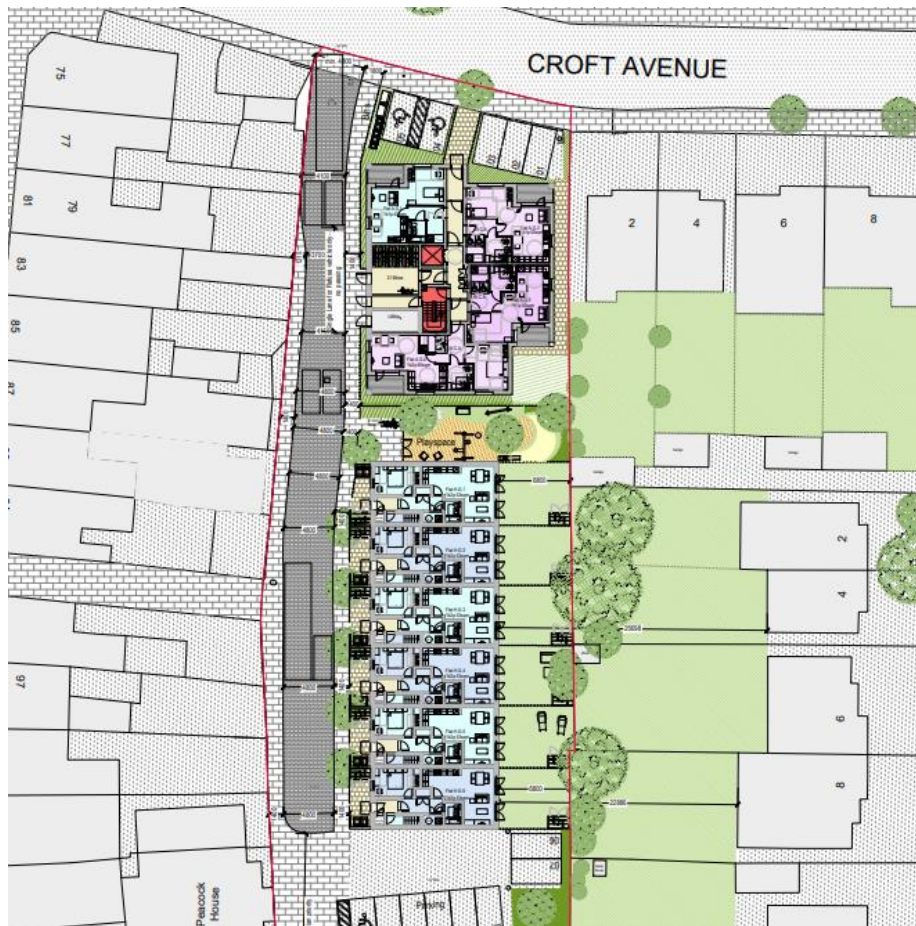


Fig.23 – Proposed Housing Ground Floor Plan Site Layout

6.6.8 Whilst the proposed apartment housing Block A would be visible from the rear of the properties on Oak Grove, the separation distance and oblique views from the proposed windows and balconies would mitigate the impact on outlook and privacy to these neighbouring residents.

6.6.9 The separation provided to the residential units on the upper floors of the properties on Station Road and the properties on the opposite side of Croft Avenue would limit the impact on outlook and privacy to these neighbouring occupiers.

Daylight, Sunlight and Overshadowing (acceptable)

6.6.10 The Vertical Sky Component (VSC) assessment results for the surrounding windows, as indicated within the accompanying Daylight & Sunlight Assessment (prepared by eb7 dated 17th September 2021), show that all windows serving habitable rooms will retain good levels of daylight in line with (or in some cases in excess of) BRE targets (i.e. above 27% VSC or 0.8 times of their former values). Further NSC (daylight) assessment (where relevant) has also shown that all habitable rooms will retain good levels of daylight distribution, in excess of BRE targets.

6.6.11 A ground floor kitchen secondary window within the side of No. 2 Croft Avenue will see a 90% reduction to its daylight, however, as this room is dual aspect

and the main window fully adheres to BRE targets, it is considered that this property would be BRE compliant.

6.6.12 The results of the Annual Probable Sunlight Hours (APSH) sunlight assessment show that all south-facing neighbouring main living rooms will achieve high levels of sunlight, many in excess of standard BRE targets.

6.6.13 The overshadowing results show that the proposed development would have a minimal impact on the amount of overshadowing experienced by the surrounding garden amenity spaces, therefore satisfying the overshadowing requirements of the BRE Guidelines.

General Noise and Disturbance (acceptable)

6.6.14 Concerns have been raised with regards to the potential noise and disturbance to neighbouring properties caused by the playground area. Whilst it is noted that the nature of children playing outdoors may result in some additional noise, this is not considered to be significantly different from that of children playing in a private garden area given the size of the playspace.

Other Matters

6.6.15 Comments were received from local residents in respect of the consultation for the application being inadequate. However, in line with the requirements of The Town and Country Planning (development Management Procedure (England) Order 2015 (as amended) all adjoining owners were notified by letter on 01.12.21 and 13.12.21. In addition, site notices were displayed at the application site on 03.12.21 and a press advert was published in the local News Shopper on 15.12.21.

6.6.16 Concerns have been raised locally with regards to Right to Light; however, this is a private legal matter outside of the planning process.

6.6.17 Representations from neighbouring residents regarding the devaluation of existing properties, the use of public money for the development and demand for the safe extension or public hall space would not be material planning considerations.

6.7 Transport and Highways - Acceptable

6.7.1 Paragraph 105 of the NPPF requires significant development to be focused on locations which are or can be made sustainable, through limiting the need to travel and offering a genuine choice of transport modes.

6.7.2 Policy T1 of the London Plan advises that development proposals should facilitate the delivery of the Mayor's strategic target of 80 per cent of all trips in London to be made by foot, cycle or public transport by 2041.

Trip Generation (acceptable)

- 6.7.3 The application is supported by a Transport Statement (prepared by Caneparo Associates dated October 2021) which states that in order to assess the potential number of trips associated with the proposed residential use, the TRICS database for affordable flats has been utilised, located within Greater London only and of a similar size development to the proposals. A summary is included in the Table below, with a focus on the weekday AM (08:00-09:00) and PM (17:00-18:00) peak hours.

| Time Period | Total Person Trip Rates (Per Unit) | | | Total Person Trips (Based on 26 Units) | | |
|-----------------------|---------------------------------------|-------|-------|---|-----|-------|
| | In | Out | 2-Way | In | Out | 2-Way |
| AM Peak (08:00-09:00) | 0.131 | 0.869 | 1.000 | 3 | 23 | 26 |
| PM Peak (17:00-18:00) | 0.355 | 0.257 | 0.612 | 9 | 7 | 16 |

Fig.24 Table TRICS Trip Rates and Trip Generation – Proposed Residential Use
(Table 5.1 within accompanying Transport Statement)

- 6.7.4 As can be seen in the table above, the proposed residential use has the potential to generate 26 two-way person trips across the morning peak hour and 16 two-way trips across the evening peak hour.
- 6.7.5 The modal split for the proposed residential use, as outlined previously in above Table, has been applied to the trip generation assessment with a summary provided in the Table below.

| Mode | Split | AM Peak | | | PM Peak | | |
|------------------|-------------|----------|-----------|-----------|----------|----------|-----------|
| | | In | Out | 2-Way | In | Out | 2-Way |
| Underground | 2% | 0 | 0 | 1 | 0 | 0 | 0 |
| Train | 32% | 1 | 7 | 8 | 3 | 2 | 5 |
| Bus | 8% | 0 | 2 | 2 | 1 | 1 | 1 |
| Taxi | 0% | 0 | 0 | 0 | 0 | 0 | 0 |
| Motorcycle | 2% | 0 | 0 | 0 | 0 | 0 | 0 |
| Driving a car | 45% | 2 | 10 | 12 | 4 | 3 | 7 |
| Passenger in car | 3% | 0 | 1 | 1 | 0 | 0 | 0 |
| Bicycle | 1% | 0 | 0 | 0 | 0 | 0 | 0 |
| On foot | 7% | 0 | 1 | 2 | 1 | 0 | 1 |
| Total | 100% | 3 | 23 | 26 | 9 | 7 | 16 |

Note: Figures subject to rounding

Fig.25 Proposed Trip Generation by Mode (Residential)
(Table 5.2 within accompanying Transport Statement)

- 6.7.6 There will be approximately 12 and 7 additional car trips in the AM and PM peak periods respectively. This equates to one additional vehicle every 5 – 9 minutes. It is noted that the existing site comprises a public car park which generates more vehicle trips than the proposed development. As such, the proposals are considered to provide a net-benefit on the local highway network.
- 6.7.7 It is predicted that the proposed residential units at the site will result in an additional 8 and 5 two-way rail trips in the AM and PM peaks respectively. When based on the total number of services operating in the vicinity of the site (8 services an hour in each direction, therefore a total of 16), this equates to 1 additional person per every 2-3 services, the effect of which is expected to be minimal.
- 6.7.8 There is expected to be 2 additional bus trips in the AM peak and 1 additional bus trip in the PM peak. When based on the total number of services operating in the vicinity of the site (5 services an hour in each direction, therefore a total of 10), this equates to 1 additional passenger per 5-10 services. The accompanying Transport Statement therefore concludes that the level of impact on buses services and public transport services generally will be negligible and fall within daily fluctuations.

Access (acceptable)

- 6.7.9 The existing access into the car park from Croft Avenue will be made redundant and reinstated as footway with vehicular access created to the parking spaces in front of Block A and the main access being from the existing service road to the western side of the site, which will be improved as part of the proposals.
- 6.7.10 The improved access road will measure 4.8m in width with a 1.8m wide footway on the eastern side. The access road then varies in width from 3.7m to 4.8m with the footway along the eastern side present for its full length. A footway would also be present along part of the western side to provide access to the rear of the row of commercial properties that front Station Road, as well as some residential units to the rear of these Station Road properties including Peacock House.
- 6.7.11 The access road would provide an improved pedestrian route through the site for both the proposed new residential units within the site as well as these existing residential units outside the site to the west. It would also provide a shared vehicular and pedestrian access to the proposed 8 off street parking spaces to the south of the proposed Mews Housing. Shared pedestrian and vehicular access to the private car park to the rear of No. 115 Station Road, which is not part of the application site, is also shown to be maintained.
- 6.7.12 The width of the access road would ensure that two-way vehicle movement is feasible for most of its length, with sufficient space for two cars to wait at the access which ensures that there will no queuing back onto Croft Avenue which is acceptable. A junction visibility assessment has been undertaken for the

access which demonstrates that a 2.4m x 43m visibility is achievable which is based on the speed limit of the road (30mph), which is acceptable.

6.7.13 The accompanying vehicle swept path analysis indicates that Bromley's refuse vehicle and delivery vehicles can turn within the site, so that these vehicles are able to access/egress the site in forward gear. This ensures that bin dragging distances are minimised for the Waste collection operatives, whilst vehicles can also wait on Croft Avenue adjacent to the communal bin store.

6.7.14 It is proposed for this access road to become adopted and it would require Private Street Works (PSW) which needs to be carried out under section 228 of the Highway Act 1980. The Council's Highways Officer has advised that the PSW must be secured by way of a condition on any approval, as it is not possible for these works to be dealt with by way of a S106 agreement given that the applicant is also the London Borough of Bromley. The detailed design work for the access road would also need to be submitted through this condition prior to the commencement of its construction to ensure it is built to adoptable standards.

Car Parking – Residential (acceptable)

6.7.15 Policy T6 of the London Plan requires developments to provide the appropriate level of car parking provision stating that car-free development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport, with developments elsewhere designed to provide the minimum necessary parking ('car-lite'). Policy T6.1 of the London Plan outlines the maximum car parking standards for residential development.

6.7.16 Policy T6.1 G also sets the requirements for adequate residential disabled car parking provision; a minimum of 3% of dwellings (with at least one designated disabled parking bay) provided from the outset and how an additional 7% of dwellings could be provided with disabled parking as soon as the existing provision is insufficient.

6.7.17 There is concern raised locally that the number of car parking spaces is insufficient. However, 13 car parking spaces, including 3 disabled bays (a total of 10% of dwellings) are proposed for the residential units which is in line with London Plan Policy T6.1. Of these 13 spaces, 5 spaces (including 2 designated disabled bays) are located at the front of Block A with direct access from Croft Avenue and 8 spaces (including 1 designated disabled bay) are located in a small parking area to the south of the Mews housing accessed via the new road along the west of the site leading from Croft Avenue.

6.7.18 In line with Policy T6.1 B, communal car parking spaces should be leased, not sold. As such, arrangements for assigning car parking spaces should be within a Car Parking Management Plan which should also detail how spaces will be repurposed into ancillary space if the spaces are unused. This can be secured by way of a condition on any approval.

6.7.19 Policy T6.1 also requires at least 20 per cent of spaces to have active electric charging facilities, with passive provision for all remaining spaces. The accompanying Transport Assessment and Planning Statement indicate that this policy requirement will be complied with; however, no specific details have been provided. A condition could be placed on any approval to ensure this requirement is met.

Car parking – Library (acceptable)

6.7.20 There are no maximum car parking standards within the London Plan specifically for the library. However, disabled parking for non-residential use should adhere to Policy T6.5 of the London Plan. This requires access to at least one on or off street disabled space.

6.7.21 It is acknowledged that the existing public car park to the rear of the library would be lost as part of the housing element of the development and that at present this is likely used by people visiting the library given its location. However, as outlined above, the submitted Transport Statement and supporting parking survey demonstrates that there are alternative public car parks in West Wickham which can accommodate the displaced demand. In addition, there is one existing on-street disabled bay immediately in front of the library building (to the south) on Glebe Way which would accord with Policy T6.5, and a row of existing short stay parking bays to the south of the library on Glebe Way which would provide some short-stay parking close to the library.

Cycle Parking – Residential (acceptable)

6.7.22 Cycle parking should be in line with Policy T5 of the London Plan, and the quality should follow the London Cycle Design Standards (LCDS).

6.7.23 A total of 47 cycle parking spaces are proposed, of which 45 will be long stay spaces and 2 will be short stay visitor spaces. 21 cycle spaces would be provided within a designated storage area on the ground floor of Block A for the occupants of the flats – 20 of these would be provided in Josta two-tier racks. Cycle stores to provide parking for two bicycles are also proposed within both the front and rear gardens of each of the Mews Housing providing two spaces per unit. These are indicated to be sheltered and secure.

6.7.24 The two visitor cycle spaces would be provided in the form of a Sheffield stand located between Block A and the Mews Housing, adjacent to the communal play area.

Cycle parking – Library (acceptable)

6.7.25 Both long-stay and short-stay cycle parking for the library use will also need to be provided, in line with Policy T5.

6.7.26 There are 8 existing sheltered cycle parking spaces located on Glebe Way adjacent to the library which are shown to be retained in relation to the library use.

6.7.27 The Council's Highways Officer has advised that the cycle parking for both the residential development library is acceptable.

Deliveries, servicing and construction (acceptable)

6.7.28 All servicing and deliveries for the residential development can take place on-site with access from Croft Avenue, as the width of the access road ensures that a car can still pass a delivery vehicle if loading/unloading.

6.7.29 The accompanying Transport Statement states that the majority of deliveries to the proposed residential units are expected to be undertaken by small to medium sized vehicles, with the occasional requirement for larger vehicles. Based on survey information contained with the TRICS database, residential developments generate around 8 or 9 delivery/collections per 100 units per day on average. Therefore, based on this, the Transport Statement concludes that the development is likely to generate 2-3 additional delivery/collection trips per day, on average, the level of impact of which will be immaterial and will fall within daily fluctuations. Furthermore, the delivery will most likely form part of an existing trip and will therefore already be travelling on the local highway network.

6.7.30 In line with Policies T4 and T7 of the London Plan, a full Delivery and Servicing Plan is required for this development which can be provided by way of a condition on any approval.

6.7.31 To ensure the impact on the highway network and on neighbouring amenity is managed during the construction process, a construction logistics and environmental management plan is considered necessary and reasonable on any approval in accordance with London Plan Policy T7 and Local Plan Policy 31.

Refuse/Recycling (acceptable)

6.7.32 The refuse for the library will remain as existing, located to the eastern side of the building.

6.7.33 A communal residential refuse and recycling store for use by residents of the flats within Block A is proposed to the front of the block adjacent to the pavement on Croft Avenue and the new pavement for the new access road. 2 no. 1360l wheely bins for refuse and 5no. 240l wheely bins for recycling are proposed.

6.7.34 Separate bin stores containing two no. 360l bins are proposed within the front garden area of each of the Mews buildings for use by the occupants of the two units within each building. Residents will be responsible for transferring their waste from their household into the correct bins.

6.7.35 No details have been provided of the bin enclosures. However, these could also be required by condition to ensure their size and design is appropriate.

6.7.36 As indicated above, the access arrangements for refuse vehicles are considered acceptable.

6.8 The Natural Environment - Acceptable

6.8.1 Paragraph 174 of the NPPF outlines that planning policies and decisions should contribute to and enhance the natural and local environment by recognising the intrinsic character and beauty of the countryside, and the wider benefits from natural capital and ecosystem services – including the economic and other benefits of the best and most versatile agricultural land, and of trees and woodland; and by minimising impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures. This is reflected in the Valued Environments Policies of the Bromley Local Plan.

Urban Greening and Trees (acceptable)

6.8.2 Policy G5 (Urban greening) of the London Plan outlines that major development proposals should contribute to the greening of London by including urban greening by including urban greening as a fundamental element of site and building design.

6.8.3 The application is accompanied by a Landscape Plan (drawing no. 2023-1150) and Urban Greening Site Plan (drawing no. 2023-1110 REV A). The submitted documents demonstrate that the proposal would achieve an Urban Greening Factor of 0.4 which satisfies the minimum recommendations outlined within Policy G5 for a predominantly residential development.

6.8.4 The agent has clarified that the Urban Green calculations for plant species are based on the typical description within the matrix and as such details of the species to be used and the long-term management of the greening should be secured by an appropriate condition on any approval.

6.8.5 A condition requiring the size and species of all trees should also be secured by condition to ensure that a suitable choice is made, particularly for the feature tree on the corner.

6.8.6 The application has identified three existing trees at the site. Trees A and B are located on the eastern boundary to the rear of 6 Oak Grove and are shown to be retained. Tree C is a street tree located on Croft Avenue and would need to be re-located or replaced to allow the entrance to the proposed parking spaces at the front of housing Block A.

Biodiversity (acceptable)

6.8.7 London Plan Policy G6 (Biodiversity and access to nature) states that proposals that create new or improved habitats that result in positive gains for biodiversity should be considered positively. Policy G6 Part D further advises that “*Development proposals should manage impacts on biodiversity and aim to*

secure net biodiversity gain. This should be informed by the best available ecological information and addressed from the start of the development process.”

- 6.8.8 Policy 72 of the Local Plan states that planning permission will not be granted for development or change of use of land that will have an adverse effect on protected species, unless mitigating measures can be secured to facilitate survival, reduce disturbance or provide alternative habitats.
- 6.8.9 The DEFRA Biodiversity Metric has been used to calculate a substantial score of 639.71% in terms of biodiversity net gain. A full copy of this metric was submitted on 26.01.22 to accompany the application to demonstrate how this would be achieved.
- 6.8.10 In line with the recommendation from the RPSB, a planning condition requiring 26 swift nest bricks to be installed can also be secured.

6.9 Energy and Sustainability – Acceptable

- 6.9.1 Policy SI 2 of the London Plan - Minimising greenhouse gas emissions states that Major development should be net zero-carbon, meaning reducing greenhouse gas emissions in operation and minimising both annual and peak energy demand in accordance with the following energy hierarchy:
- 1) be lean: use less energy and manage demand during operation
 - 2) be clean: exploit local energy resources (such as secondary heat) and supply energy efficiently and cleanly
 - 3) be green: maximise opportunities for renewable energy by producing, storing and using renewable energy on-site
 - 4) be seen: monitor, verify and report on energy performance.
- 6.9.2 A minimum on-site reduction of at least 35 per cent beyond Building Regulations is required for major development. Residential development should achieve 10 per cent, and non-residential development should achieve 15 per cent through energy efficiency measures.
- 6.9.3 Where it is clearly demonstrated that the zero-carbon target cannot be fully achieved on-site, any shortfall should be provided, in agreement with the borough, either:
- 1) through a cash in lieu contribution to the borough's carbon offset fund, or
 - 2) off-site provided that an alternative proposal is identified, and delivery is certain.
- 6.9.4 In 2019, the London Borough of Bromley also approved a ten-year plan to ensure that the Council will have net zero emissions by 2029. The commitment is one of the most ambitious targets of any London borough. Work to move towards the net zero emission target will include tree planting, an energy efficiency programme, expanding renewable energy and LED street lighting, and other initiatives.

6.9.5 The proposed energy efficiency measures to be incorporated would include;

- enhancements to the building fabric envelope thermal performance;
- provision of new high-performance glazing to reduce solar heat gains;
- maximising the use of daylighting within the buildings;
- reduction of CO2 emissions through installation of high efficient heat pumps;
- installation of a 4.5kWp photovoltaic system on the roof of the library and a 12kWp photovoltaic system on the roof of housing block A;
- minimising overheating through design of orientation, fenestration, insulation; and
- minimising overheating through the use of energy efficient lighting systems, high ceilings and passive ventilation.

6.9.6 The application is accompanied by an Energy Statement prepared by Energytest (September 2021) which states that the above measures would achieve a total site wide reduction of 57% with 39% just from renewables (Be Green) and an additional 18% from fabric-first and energy efficient systems (Be Lean).

6.9.7 The Council's Energy Officer has been consulted and no objection is raised to the proposal. A condition is recommended to secure the carbon saving measures as set out in the energy statement.

6.9.8 A total carbon offsetting payment-in-lieu of £59,195.00 has been recommended to be secured.

6.10 Drainage - Acceptable

6.10.1 Policy 116 of the Bromley Local Plan (2019) states that all developments should seek to incorporate Sustainable Urban Drainage Systems (SUDS) or demonstrate alternative sustainable approaches to the management of surface water as far as possible. This is supported by Policy SI 13 (Sustainable Drainage) of the London Plan (2021).

6.10.2 The site lies within Flood Zone 1. The application has been accompanied by a Flood Risk & Sustainable Drainage Assessment (prepared by parmabrook dated 7th October 2021) and a Foul & Surface Water Drainage Assessment (prepared by parmabrook dated 14th September 2021).

6.10.3 The Council's Drainage Officer and Thames Water have raised no objections to the proposed development subject to informatics and a condition requiring the submission of the detailed design measures as stated within the submitted Flood Risk & Sustainable Drainage Assessment to be imposed on any approval.

6.11 Technical Matters - Acceptable

Air Quality (acceptable)

6.11.1 Policies SI 1 of the London Plan and 120 of the Bromley Local Plan detail the need to tackle poor air quality.

6.11.2 The site lies within an Air Quality Management Area (AQMA) declared for NO_x. The application is accompanied by an Air Quality Assessment (prepared by Anderson Acoustics revised January 2022) which concludes that the proposed development can be considered air quality neutral and air quality positive.

6.11.3 The Council's Environmental Health Officer has advised that any approval should include a condition requiring any gas boilers to meet a dry NO_x emission rate of <40mg/kWh to minimise the effect of the development on local air quality within an Air Quality Management Area, as well as a condition requiring all Non Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases of the development to comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance 'Control of Dust and Emissions During Construction and Demolition' dated July 2014 (SPG) or any subsequent guidance, should also be imposed on any approval.

6.11.4 The construction phase will have the potential to create dust and dust mitigation and management measures should be included with a Construction and Environmental Management Plan.

Contaminated Land (acceptable)

6.11.5 Policy 118 of the Bromley Local Plan states that where the development of contaminated land, or land suspected of being contaminated, is proposed, details of site investigations and remedial action should be submitted.

6.11.6 The application has not been supported by a contaminated land assessment, but the application form states that part of the site is expected to be on contaminated land. Therefore, the Council's Environmental Health Officer has advised that a condition requiring the submission of a contaminated land assessment and associated remedial strategy, together with a timetable of works, prior to the commencement of any development on site should be included on any approval.

Noise (acceptable)

6.11.7 London Plan Policy D14 (Noise) states that development should reduce, manage and mitigate noise to improve health and quality of life. Policy D13 (Agent of Change) places the responsibility for mitigating impacts from existing noise and other nuisance-generating activities or uses on the proposed new noise sensitive development. Development should be designed to ensure the established noise and other nuisance-generating uses remain viable and can continue or grow without unreasonable restrictions being placed on them. This is supported by Bromley Local Plan Policy 119.

6.11.8 The application has been accompanied by an Environmental Noise Survey and Acoustic Design Statement Report (prepared by Hann Tucker Associates dated 29th September 2021) which concludes that the site, subject to appropriate mitigation measures, is suitable for residential development in terms of noise. The Council's Environmental Health Officer has advised that a condition requiring full details of a scheme of noise mitigation measures in full compliance with all the recommendations within this acoustic report should be included on any approval.

6.12 Planning Obligations and CIL

Legal Agreement Heads of Terms and Transfer of funds

6.12.1 The National Planning Policy Framework (NPPF) states that in dealing with planning applications, local planning authorities should consider whether otherwise unacceptable development could be made acceptable through the use of conditions or planning obligations. Planning obligations should only be used where it is not possible to address unacceptable impacts through a planning condition. It further states that where obligations are being sought or revised, local planning authorities should take account of changes in market conditions over time and, wherever appropriate, be sufficiently flexible to prevent planned development being stalled. The NPPF also sets out that planning obligations should only be secured when they meet the following three tests:

- (a) Necessary to make the development acceptable
- (b) Directly related to the development; and
- (c) Fairly and reasonably related in scale and kind to the development.

6.12.2 Policy 125 of the Bromley Local Plan (2019) and the Council's Planning Obligations SPD state that the Council will, where appropriate, enter into legal agreements with developers, and seek the attainment of planning obligations in accordance with Government Guidance.

6.12.3 The development, as proposed, would necessitate the following obligations, which have been agreed:

- Carbon offset cash-in-lieu: £59,195.00

6.12.4 As the Council is unable to enter into a planning obligation with itself; as both Applicant and the Local Planning Authority, the Applicant has confirmed the required planning obligations; for carbon offsetting to mitigate the impact of the proposal development, will be transferred to the Council's funding and delivery programmes prior to the planning decision being issued.

6.12.5 These obligations meet the statutory tests set out in Government guidance, i.e. they are necessary, directly related to the development and are fairly and reasonably related in scale and kind to the development.

6.12.6 A condition will also be added to the scheme to ensure that if any landowner have the ability to enter into a section 106 agreement requiring any purchasers

of the site to enter into legal agreement to ensure that the conditions which would usually be secured via legal agreement.

CIL

6.12.7 The London Borough of Bromley Community Infrastructure Levy (CIL) proposals were approved for adoption by the Council on 19 April 2021, with a date of effect on all relevant planning permissions determined on and after 15 June 2021. The Bromley Community Infrastructure Levy (CIL) allows the Local Authority to raise funds from new development towards local infrastructure.

6.12.8 The Mayor of London's CIL is also a material consideration. CIL is payable on this application and the applicant has completed the relevant form.

6.12.9 As such, and notwithstanding third party comments, the proposal would contribute towards supporting local infrastructure and services.

7 CONCLUSION

7.1 As the Council cannot at present demonstrate a 5 year land supply of deliverable housing sites, the housing policies of the development plan are out-of-date and the presumption of sustainable development set out in Para. 11 of the NPPF applies to the application. This means a presumption in favour of granting planning permission, unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies within the NPPF taken as a whole. There are no other adverse impacts of the scheme that are considered to significantly and demonstrably outweigh the economic, social and environmental benefits of the scheme when considering the NPPF as a whole. The balance test is therefore tilted towards granting planning permission.

7.2 The principle to redevelop the site including the enhancement of library provision, loss of the public car park, and introduction of residential units is supported at this town centre location.

7.3 The proposed redevelopment of the library would provide an enhanced provision of community service(s) and its design and appearance would respect that of the streetscene and area within which it lies.

7.4 The proposed development would deliver 26 residential dwellings, including 14 affordable rent units, which would represent a moderate contribution to the supply of housing within the Borough, at a time when there is a significant under supply.

7.5 The proposed layout of the dwellings would offer a good quality of internal amenity alongside a suitable provision of external amenity spaces and childrens play area, and would have an acceptable impact on the neighbouring residential amenities in terms of daylight/sunlight conditions, privacy and outlook.

- 7.6 The proposal would provide sufficient and appropriately laid out car parking for disabled persons, bicycle and refuse/recycling storage.
- 7.7 The proposal has demonstrated a reasonable attempt to reduce carbon dioxide emissions on the site/within the development and that the remaining carbon reduction could be managed through a payment in lieu to offset the outstanding reduction. The development would not have adverse Environmental Health or drainage and flooding effects.
- 7.8 The proposal would achieve an Urban Greening Factor of 0.4 and a 639.71% biodiversity net gain at the site.
- 7.9 As set out in the report, the scale and design of the proposed flatted block (Block A) is considered to be out of keeping with the character and appearance of Croft Avenue within which it would lie. However, this could to some extent be mitigated by appropriate quality materials secured by condition. In addition, it is accepted that the number of dwellings provided within Block A, all of which are to be affordable social rented, are only possible by way of the size and design of the building proposed.
- 7.10 Officers therefore conclude that the benefits of the scheme clearly outweigh the harm identified. As such, the proposals would comply with both the Framework and the development plans taken as a whole. The balance test is therefore tilted towards granting planning permission and the scheme is considered acceptable overall.
- 7.11 Subject to compliance with the recommendations in the technical reports and implementation of the recommended works undertaken where necessary, it is considered that the application should be approved, subject to planning conditions.
- 7.12 Background papers referred to during production of this report comprise all correspondence on the files set out in the Planning History section above, excluding exempt information.

RECOMMENDATION: GRANT PLANNING PERMISSION SUBJECT TO TRANSFER OF FUNDS AND PLANNING CONDITIONS.

SUMMARY OF CONDITIONS AND INFORMATIVES

Standard Conditions:

1. Time limit of 3 years
2. Approved drawing numbers

Pre-Commencement Conditions:

3. Contaminated Land
4. Construction Logistics and Environmental Management Plan

5. Details of design measures in Flood Risk & Sustainable Drainage Assessment Report
6. Slab levels and ridge heights

Above Ground Construction Conditions:

7. S278 condition for new access road
8. External materials
9. Public realm design (including boundary treatments, public square, child play equipment and a management plan, lighting)
10. Scheme of landscaping (hard and soft)
11. Secure By Design
12. Details of noise mitigation measures
13. Surface Water discharge to Highway
14. Full details of storage of refuse/recycling
15. Full details of bicycle parking storage
16. Privacy screening
17. Urban Greening Factor details of species and long-term management of greening
18. Details of biodiversity enhancements
19. Swift nest bricks

Prior to Occupation Conditions:

20. Car Parking Management Plan
21. Delivery and Servicing Plan
22. Electric charging points
23. Provision of adequate water supplies for firefighting purposes
24. Access for fire appliances
25. Plant noise

Compliance Conditions:

26. Retention of car parking spaces
27. Affordable Housing
28. Early stage viability review
29. Legal locus condition
30. Compliance with M4(2) and M4(3) dwellings
31. Compliance with Fire Statement
32. Compliance with Energy Statement
33. Compliance with Air Quality Assessment
34. All Non-Road Mobile machinery to comply with relevant emissions standards
35. Low NOx boilers
36. No PD upward extensions

Any other planning condition(s) considered necessary by the Assistant Director of Planning

Informatives

- CIL

- Thames Water (various)
- CEMP to be prepared inline with the Council's control of pollution and noise from demolition and construction sites- code of Practice Dust Monitoring.
- Street name and numbering

Any other informative(s) considered necessary by the Assistant Director of Planning



This page is left intentionally blank

Report No.
DRR000000

London Borough of Bromley

PART ONE - PUBLIC

Decision Maker: **DEVELOPMENT CONTROL COMMITTEE**

Date: **19 April 2022**

Decision Type: Non-Urgent Non-Executive Non-Key

Title: **HERITAGE AT RISK UPDATE**

Contact Officer: Ben Johnson, Head of Planning Policy and Strategy
E-mail: ben.johnson@bromley.gov.uk

Simon Went, Principal Conservation Officer
E-mail: simon.went@bromley.gov.uk

Chief Officer: Tim Horsman, Assistant Director (Planning)

Ward: Biggin Hill; Bromley Common and Keston (Pre 2022); Bromley Town;
Chislehurst; Cray Valley East (Pre May 2022); Hayes and Coney Hall; Penge
and Cator; Plaistow and Sundridge (Pre May 2022);

1. Reason for report

- 1.1. The Heritage at Risk Register includes historic buildings and sites at risk of being lost through neglect, decay or deterioration. There are currently 25 heritage assets within Bromley that are included on the latest iteration of the register. This report provides an update on these assets where information is available.
- 1.2. This report was prepared for the meeting of the Renewal, Recreation and Housing Policy Development and Scrutiny Committee (RRHPDS) on 16 November 2021. The update in Table 1 reflects the known situation with Bromley's 'Heritage at Risk' assets as of November 2021. An extract from the minutes of the RRHPDS meeting on 16 November 2021 is provided at Appendix 1.

2. RECOMMENDATION(S)

- 2.1. That the Development Control Committee note the updates on various sites within Bromley that are included on the Historic England 'Heritage at Risk' register.

Impact on Vulnerable Adults and Children

1. Summary of Impact: No impact
-

Corporate Policy

1. Policy Status: Not Applicable
 2. BBB Priority: Regeneration
-

Financial

1. Cost of proposal: N/A
 2. Ongoing costs: N/A
 3. Budget head/performance centre: Planning Policy and Strategy
 4. Total current budget for this head: £0.568m
 5. Source of funding: Existing Revenue Budget for 2022/23
-

Personnel

1. Number of staff (current and additional): 1
 2. If from existing staff resources, number of staff hours: N/A
-

Legal

1. Legal Requirement: the 'Heritage at Risk' register is a non-statutory programme undertaken by Historic England.
 2. Call-in: Not Applicable
-

Procurement

1. Summary of Procurement Implications: N/A
-

Customer Impact

1. Estimated number of users/beneficiaries (current and projected): N/A
-

Ward Councillor Views

1. Have Ward Councillors been asked for comments? No
2. Summary of Ward Councillors comments: N/A

3. COMMENTARY

- 3.1. The Heritage at Risk Register is an annual register produced by Historic England (HE). It lists historic buildings and sites at risk of being lost through neglect, decay or deterioration, and includes all types of designated heritage assets, including Conservation Areas.
- 3.2. The aim of the Register is to focus attention on those places in greatest need. HE work in partnership with local authorities and others to help find imaginative solutions for historic places and sites at risk across England.
- 3.3. The vast majority of heritage assets in Bromley are well looked after; however, a small proportion (25 heritage assets) are considered to be at risk and are included on the latest register¹
- 3.4. The reasons why a heritage asset is included can vary, but commonly includes assets that are long-term vacant; neglected and/or poorly maintained; structurally unsound; damaged by fire; unsecured and in danger of loss; or threatened with demolition. The cost of repairing heritage assets at risk can often be significant, which can be another barrier to repair.
- 3.5. Certain statutory powers - Urgent Works Notice and Repairs Notice - are available to the Council, but the preference is to work with owners and stakeholders to seek positive outcomes. The use of such statutory powers could also involve significant financial risks to the Council, including compulsory purchase, although HE can provide significant grant funding² to underwrite up to 80% of the costs of carrying out such enforcement action.
- 3.6. Bromley Council monitors assets at risk, in conjunction with HE, site owners, friends' groups, developers and other stakeholders, proactively identifying opportunities to address Heritage at Risk repairs and find sustainable long-term solutions for these assets across the Borough. Table 1 below lists the current identified assets at risk and provides an update (where available). The Council's Regeneration team has built a good relationship with HE, working closely and constructively with HE officers on the Heritage at Risk agenda in Bromley for several years; this has enabled the Council to benefit from extensive expert advice from HE. Many of the current entries on the Heritage at Risk Register are difficult, long-term entrenched cases. HE has welcomed the Council's pro-active approach to tackling these cases, notably at Crystal Palace Park. Here, the masterplan-driven approach has facilitated excellent collaboration, enabling HE to contribute significant funding to key elements, such as the Subway, in the knowledge that this is part of an on-going commitment to the whole park, and that HE's investment of public funding through their grants programme will be safe-guarded and public value maximised.
- 3.7. Similar collaboration is taking place on the Council-owned heritage assets within Bromley Palace Park, and HE have noted that this also promises a good outcome. However, HE have expressed disappointment that an opportunity was missed to address the Grade II listed Victorian Folly as part of a recent land transaction.
- 3.8. HE is also keen to support the Council in taking enforcement action where this is essential to move cases forward and can offer grant assistance for unrecoverable costs. West Camp at Biggin Hill remains a major concern, although it has great potential. HE will provide continuing support to the Council to resolve such cases, to ensure that the privately-owned, vacant buildings are repaired and brought back into use.

¹ Historic England, Heritage at Risk register - London & South East Register 2021. published November 2021, available from: <https://historicengland.org.uk/images-books/publications/har-2021-registers/lon-se-har-register2021/>

² Information on HE grants available from: <https://historicengland.org.uk/services-skills/grants/our-grant-schemes/grants-to-underwrite-urgent-works/> and <https://historicengland.org.uk/services-skills/grants/our-grant-schemes/acquisition-grants/>

Table 1: update on heritage assets at risk in Bromley

| Heritage asset | Comments |
|--|---|
| <p><u>Assets at Crystal Palace Park</u></p> <p>Crystal Palace Park (Grade II* Listed Park)</p> <p>Prehistoric animal sculptures, geological formations and lead mine on islands in lake* - Grade I</p> <p>Upper and Lower Terraces - Grade II</p> <p>Pedestrian subway under Crystal Palace Parade - Grade II*</p> <p>North and south railings, Crystal Palace Parade - Grade II</p> <p>Base of the Southern Crystal Palace Water Tower - Grade II</p> | <p>General: Development Control Committee resolved to grant planning permission³ for an outline application at Crystal Palace Park on 25 March 2021, subject to the prior completion of a Section 106 legal agreement and any Direction by the Mayor of London and the Secretary of State. This application is intended to fund a comprehensive regeneration plan for Crystal Palace Park.</p> <p>Officers continue to hold regular meetings with HE and other internal and external stakeholders.</p> <p>Dinosaur Island: The Council is working in conjunction with the Friends of Crystal Palace Dinosaurs to implement intruder proof planting mechanisms for the weirs, to help prevent trespassing on the Island. An Access Management Plan is currently being agreed with HE, following the completion of the swing bridge in June 2021. HE has set up a self-funded expert board to directly support the future conservation of Dinosaur Island as funding for the park's regeneration plan starts to come through. A Hydrological report is complete with further recommendations for repair, although these have not been actioned yet. The Friends of Crystal Palace Dinosaurs have put in an application to the GLA Green Spaces fund to progress this further.</p> <p>Megalosaurus: Temporary repair works were completed in May 2021, which replaced damaged sections of the sculpture's broken jaw with a replica prosthesis model. The replacement will help protect the Grade I listed structure until permanent repair works are undertaken in the future.</p> <p>Pterodactyls and Megaloceros (Irish Elk): An updated condition survey and report are currently being commissioned to establish costs and appropriate repair methodologies for the three sculptures, which have sustained prolonged damage over recent years.</p> |

³ Planning application ref: 20/00325/OUT

| Heritage asset | Comments |
|---|--|
| | <p>Ichthyosaur: The Council is liaising with HE and Idverde (who deliver Bromley's outsourced Parks and Greenspace Service) to undertake repair works to the Ichthyosaur that sustained accidental damaged during routine maintenance works in August 2021. These works are being funded through Idverde's insurance.</p> <p>Crystal Palace Subway: A restricted tender process was carried out in September 2021 for the main restoration works contract, and five potential contractors were shortlisted. The second stage tender has now been released and returns are due back on 19 November 2021. It is anticipated that the construction contract will be awarded in February 2022 subject to Executive approval.</p> <p>Crystal Palace Park Railings: The restoration works for the Crystal Palace Park Railings were costed in May 2021 and submitted as part of a Levelling Up Fund (LUF) bid in June 2021. The proposed works are intended to be undertaken as the first phase to remove the conservation deficit as part of the cultural venue project, connecting to the Crystal Palace Subway. The Council is awaiting the outcome of the LUF bid.</p> <p>Base of the Southern Crystal Palace Water Tower: This asset was recently added to the statutory list and does not currently have a use. The brickwork has a coating of cement render which has fallen off in places that is now vulnerable to further weather and vegetative damage. The asset does not form part of the Crystal Palace regeneration plan.</p> |
| Ice house to Sundridge Park, Plaistow Lane, Bromley* - Grade II | <p>The Council's conservation officer has had ongoing discussions with Idverde and HE. There has also been input from Orpington and District Archaeological Society (ODAS) on the current state of the asset; ODAS noted that the asset is buried in undergrowth. Although ODAS has located the ice house, we have no knowledge of its condition and there has been no liaison on this for at least three years. It has therefore ground to a complete halt. It is worth noting that it is in the grounds of the golf course which affects access.</p> |

| Heritage asset | Comments |
|---|--|
| Old Town Hall, Tweedy Road, Bromley - Grade II | Planning permission and listed building consent granted ⁴ in March 2021 for redevelopment scheme which will reuse the vacant Grade II listed building (for office, hotel and a public house) and lead to the removal of the building from the Heritage at Risk Register. |
| <u>Biggin Hill Airport West Camp, Main Road, A233 (east side)</u> Buildings 1-5 (Airmen's barrack blocks), West Camp* - Grade II Building 10 (Junior Ranks Mess, former Airmen's Institute) - Grade II Building 12 (Candidates' Club, Former Sergeants' Mess) - Grade II Building 15 (Hawkinge Block) - Grade II Building 33 (Station HQ) - Grade II | The Council have held meetings with HE to discuss a way forward. Both the Council and HE have been in contact with the agent/owner regarding works, and some repair work has recently been carried out to the buildings, but considerably more works are required. The buildings are all vacant and have been for many years. |
| <u>Buildings at Bromley Palace Park</u> The Ice House and Ha-Ha Wall - Grade II Victorian Folly of 'Medieval Ruins' - Grade II Pulhamite waterfall - Grade II Pulhamite fernery - Grade II | The Council's conservation officer is involved in ongoing discussions relating to these assets, including with HE and colleagues in Capital Projects, Estates and Regeneration. HE undertook site visit in October 2021 to advise on repairs. Officers are awaiting an up-to-date schedule of repairs. The Victorian folly is no longer in the Council's ownership having been included in a disposal programme. |
| 91 High Street, St Mary Cray* - Grade II | Concerns relating to the condition of the house, which is very fragile. Vegetation is beginning to engulf the house and the bulging flank wall is of considerable concern. Planning Enforcement have written to the site owners to detail these concerns as a precursor to an Urgent Works Notice. The owner is absent from the property. |
| The Royal Bell Hotel, 173-177, High Street, Bromley - Grade II | Planning permission and listed building consent granted ⁵ (in June 2020 and March 2019 respectively) for redevelopment scheme including a hotel. The permission has been implemented but is not yet complete. |

⁴ Planning application ref: 19/03545/FULL and 19/03546/LBC

⁵ Planning application ref: 18/03252/FULL1 and 18/03201/LBC

| Heritage asset | Comments |
|---|---|
| Church of St John the Evangelist, High Street, Penge SE20 - Grade II | This heritage asset is currently being monitored. HE report that repairs to the roof were carried out in June 2019. |
| Biggin Hill RAF Station Conservation Area | This is currently being monitored. The West Camp buildings which are included on the heritage at risk register all fall within the Conservation Area. |
| Iron Age settlement and Roman villa at Warbank, Keston – Scheduled Monument | Substantial tree growth is threatening the site and may necessitate a management agreement with the owner to resolve. HE is monitoring the situation. Archaeological expertise and legislative control lies mainly with HE/Department for Digital, Culture, Media & Sport (DCMS). |
| Scadbury Manor moated site and fishponds - Scheduled Monument | Scadbury Manor moated site and fishponds has recently received substantial grants from both HE and Bromley Council. Continued monitoring with HE and colleagues in Capital Projects and Estates, although it should be noted that archaeological expertise and legislative control lies mainly with HE/DCMS. |
| Romano-British masonry building and Saxon cemetery, Fordcroft, Orpington - Scheduled Monument | Vandalism, and damages from utilities, are the main threats to this asset. Archaeological expertise and legislative control lies mainly with HE/DCMS. |
| Wickham Court, Layhams Road* – Grade I | Grade I Listed Building currently used as a school. The building has a large backlog of repairs, causing a tower to collapse. The Council is discussing these repairs with the owners, with a view to producing a detailed schedule of repairs needed; HE is pushing for a survey of the entire building as other areas are undoubtedly also at risk. HE is monitoring the progress of repairs, working with the owner's Surveyor. HE conducted a site visit in 2020. |

*Heritage assets which are within priority category A (Immediate risk of further rapid deterioration or loss of fabric; no solution agreed) or B (Immediate risk of further rapid deterioration or loss of fabric; solution agreed but not yet implemented). These are the highest priority categories on the register.

- 3.9. Given ongoing works or existing planning permissions, it is expected that The Royal Bell in Bromley and the Church of St John the Evangelist, High Street, Penge will be removed from the register in the short to medium term (three to five years). The buildings and structures at Crystal Palace Park, including the Registered Park, are likely to take longer to be removed, following implementation of the regeneration plan (five years plus).

4. POLICY IMPLICATIONS

- 4.1 The Bromley Local Plan sets out the objectives to continue to conserve and enhance locally and nationally significant heritage assets; and encourage a proactive approach to the protection and improvement of heritage assets to contribute to strategic, local planning and economic objectives. Decisions to proceed with repair of heritage assets sit outside the planning system, but, where such repair does proceed, it may require planning permission or listed building consent. There are several Local Plan policies that may apply to any application to repair a heritage asset at risk.

5. FINANCIAL IMPLICATIONS

- 5.1 This report provides an update on heritage assets at risk in Bromley. There are no specific financial implications arising from this report.

6. LEGAL IMPLICATIONS

- 6.1 This report provides an update on heritage assets at risk in Bromley. There are no specific legal implications arising from this report.

| | |
|---|--|
| Non-Applicable Sections: | IMPACT ON VULNERABLE ADULTS AND CHILDREN PERSONNEL IMPLICATIONS PROCUREMENT IMPLICATIONS |
| Background Documents: (Access via Contact Officer) | Historic England, Heritage at Risk register - London & South East Register 2021. Published November 2021, available from: https://historicengland.org.uk/images-books/publications/har-2021-registers/lon-se-har-register2021/ |

41 HERITAGE AT RISK UPDATE

The Committee received an update on the Heritage at Risk Register which covered historic buildings and sites at risk of being lost through neglect, decay or deterioration. There were currently twenty five heritage assets within the borough on the latest annual version of the register.

The Council was working closely with Heritage England, and there was progress in addressing the issues on many of the sites, including Crystal Palace Park, the Scadbury Manor site and the Ice House at Sundridge Park.

Some repair work had been carried out at the West Camp at Biggin Hill Airport, and there had been contact with the agent/owner, but the negotiations were difficult. A ward member suggested opening up one of the barrack blocks and establishing a heritage trail linked to the Biggin Hill Memorial Museum.

A Member commented that some of these sites had potential to attract tourism, and suggested that the Council should be prepared to invest additional funds.

RESOLVED that the update on sites within the borough that are included on the Historic England Heritage at Risk Register be noted.

This page is left intentionally blank